

07 July 2023

Your ref: 22-0279

BCD Group Tauranga  
PO Box 13276  
Tauranga Central  
Tauranga 3141

## Digitally Delivered

Dear Alex

### Decision on application for resource consent under the Resource Management Act 1991

**Application number:** LU/0023/23  
**Applicant:** Ingham Motor Group  
**Address:** 26 Lake Street Cambridge 3434  
**Legal Description:** Lot 1 DP 56389 as comprised in Record of Title SA46B/898  
Lot 2 DP 56389 as comprised in Record of Title SA46B/899  
Southern Allotment 411 Town of Cambridge East as comprised in  
Record of Title SA48/249  
**Proposed activity(s):** Use and development of a Non-complying Activity including a car dealership,  
showroom and workshop within the Cambridge B Character Precinct Area  
dispensing with various Commercial Zone performance standards

I wish to advise you of Waipā District Council's decision to **grant** your application for resource consent under the Resource Management Act 1991 (RMA). Please see below for the details of the decision and conditions of consent.

The following information provides you with some guidance on your rights and what to do next. It is recommended that you seek independent advice if you are in any doubt as to the processes to be followed.

### Objections

If you disagree with any part of this decision or any conditions of this consent, you may lodge an objection in writing to Council within **15 working days** of the receipt of this letter. Your objection must be in accordance with section 357 of the RMA and must include the reasons for your objection.

### **Compliance with conditions**

Your resource consent permits the land use to be established at the site long as the activity complies with the stated conditions on an ongoing basis. It is important that you fully understand and comply with all the conditions of your consent.

Please notify Council's monitoring department prior to the commencement of activities associated with this consent. The role of Council's monitoring department is to monitor compliance with the conditions of consent and may involve site visits.

Council's monitoring department can be contacted on [consentmonitoring@waipadc.govt.nz](mailto:consentmonitoring@waipadc.govt.nz) or 07 8233800. Please reference the consent number and address of the property when emailing or calling.

### **Lapsing of Consent/s**

This resource consent lapses five (5) years after the commencement of the consent, unless the consent is given effect to by the end of that period.

The commencement date of a resource consent is determined by section 116 of the Resource Management Act 1991.

Yours Sincerely



Quentin Budd  
**CONSENTS TEAM LEADER**

## **1 INTRODUCTION**

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The proposal includes the demolition of the existing Bunnings Warehouse building and the redevelopment of the overall site for a car dealership and associated vehicle servicing. The proposal will include three standalone single storey buildings and the utilisation of the existing vehicle crossings along Queen and Lake Street. The proposal includes illuminated pylon signage and illuminated business identification signage. The proposed total floor area of the car dealership and showroom is 2,417m<sup>2</sup>.

### **1.1 Description of site**

The subject site is located on the north side of Queen Street and on the east side of Lake Street. The site contains 3 records of title with a combined total site area of 6,852m<sup>2</sup>. The vacated Bunnings Warehouse building has a floor area of 3814m<sup>2</sup>. Cyclone fencing and car parking bays remain onsite. The majority of the subject site is hard paved with the exception of the landscaping beds and a number of established trees along Queen Street and Lake Street. Refer to Appendix C for images of existing conditions.

The surrounding sites are commercial in nature. The land to the west contains take away food and drink outlets and automotive repairs and retail businesses. To the south of the subject site is a building constructed in 2016 containing furniture retailers. To the south east of the site a Mobil service station and a Countdown supermarket present to Queen Street. To the southwest of the site is the Cambridge Town Hall and associated public open space. The Town Hall is currently used for functions and events and is historically significant to the town. To the east of the site is a Nissan car dealership and a McDonald's restaurant. North east of the site contains a mixed use commercial and residential development and cinema. As a clarification to the notification report, directly north of the site is a customer and permit-only staff car parking area as a part of the Lakewood development and a small grassed area with heritage value due to a small curved gate known as the 'kissing gate' also on the Lakewood site. Further north of the subject site is Lake Te Ko Utu and associated walkways and vegetation reserves.

The property is sited within the Commercial Zone and is subject to the Pedestrian Frontage Area Policy and Character Precinct Cambridge B of the Waipa District Plan ('District Plan'). The provisions of the Zone and the specific policy and character areas are elaborated on further in this report.

Council's Special Features Maps do not identify any special features overlaying the site.

This report will refer to Figures 1 to 4 shown below.

Figure 1: Aerial photograph of site (site shown in yellow)



Figure 2: District Plan Zone – Commercial Zone extent and the subject site shown in red.



Figure 3: District Plan Policy Overlays (Character Area B and Pedestrian Frontage Overlay)

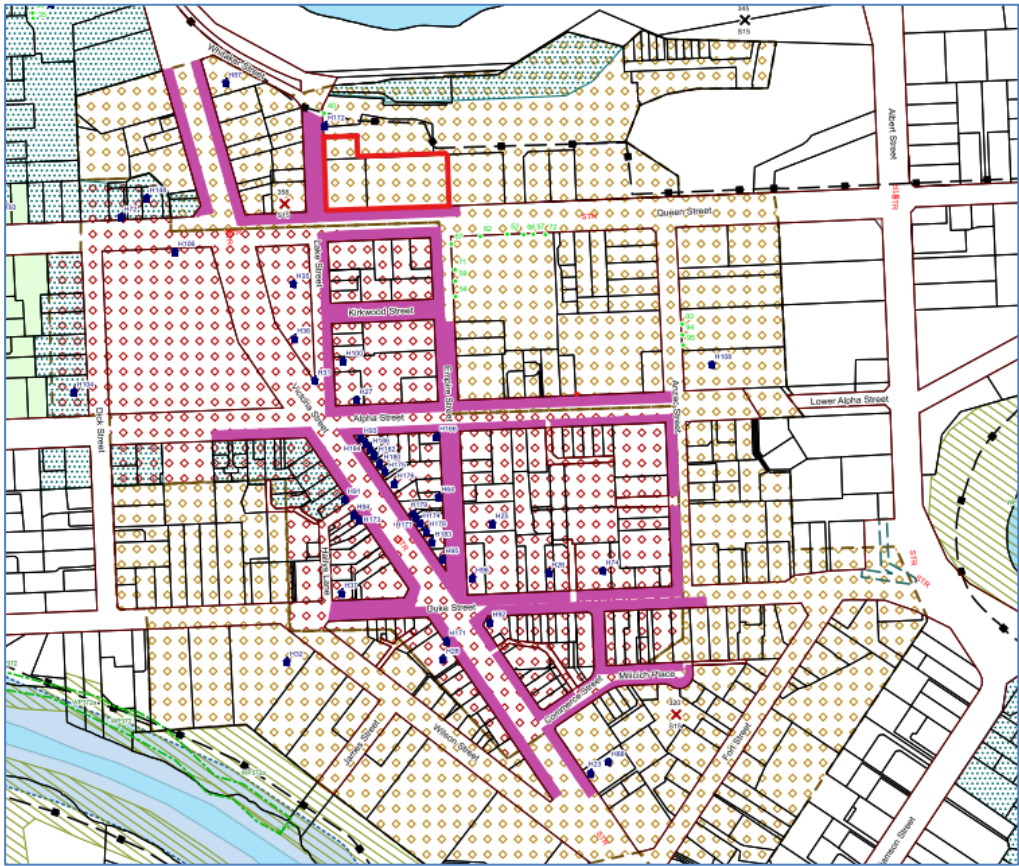
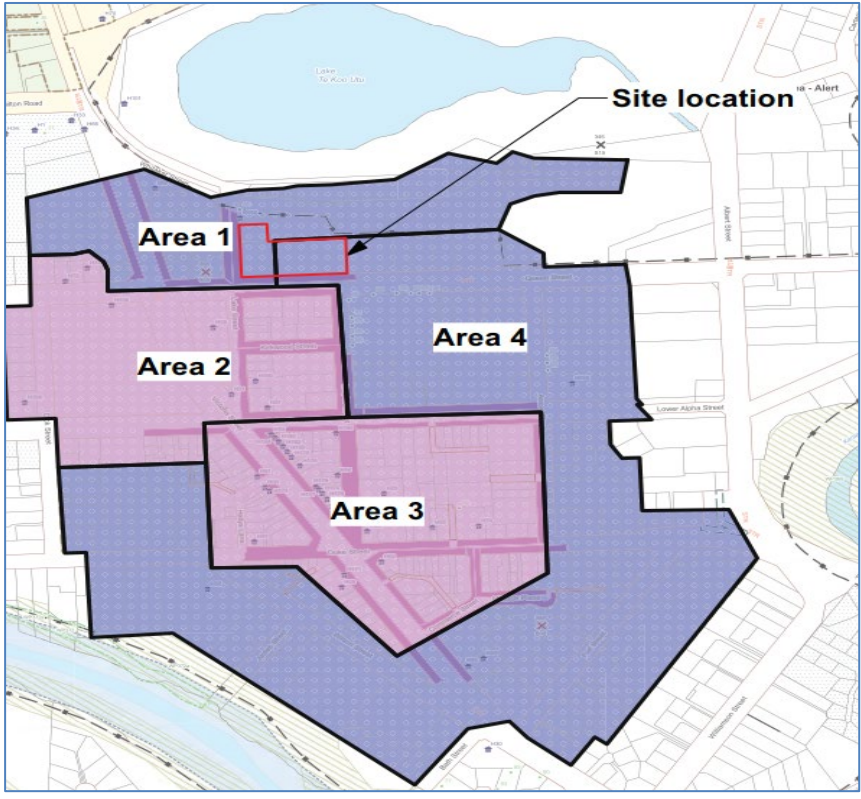


Figure 4: Character Precinct Cambridge B (Area 1 and Area 4 impacting the subject site)



## 1.2 Legal interests in the property

Table 1 below summarises the relevant interests on the existing titles.

| Title Reference | Legal Description                                     | Size               | Date Issued      | Relevant Interests |
|-----------------|---|--------------------|------------------|--------------------|
| SA46B/898       | Lot 1 Deposited Plan South Auckland 56389             | 4075m <sup>2</sup> | 5 July 1990      | ▪ n/a              |
| SA46B/899       | Lot 2 Deposited Plan South Auckland 56389             | 754m <sup>2</sup>  | 5 July 1990      | ▪ n/a              |
| SA48/249        | Southern Portion Allotment 411 Town of Cambridge East | 2023m <sup>2</sup> | 20 December 1887 | ▪ n/a              |

**Table 1:** Existing titles and interests

There are no interests of relevance on the title that would restrict the proposal from proceeding.

## 1.3 History

The property is subject to multiple resource consents, however the most recent and relevant is the Bunnings Warehouse consent which is listed in Table 2.

| Consent No. | Description  | Date Approved | Key Details   |
|-------------|--|---------------|---|
| LU/0177/08  | Application to redevelop the Bunnings site in the Town Centres Zone as a Non-Complying Activity. | 3/11/08       | <ul style="list-style-type: none"> <li>▪ Erection of a new Bunnings store encompassing a retail building, timber yard and garden centre / nursery.</li> <li>▪ Warehouse of 2,467m<sup>2</sup> gross floor area, consisting of a retail sales area, 1,236m<sup>2</sup> timber trade sales and 437m<sup>2</sup> of garden centre.</li> <li>▪ Total site coverage of 3,814m<sup>2</sup>.</li> <li>▪ Maximum wall height of 9m.</li> <li>▪ 66 onsite car parking spaces.</li> </ul> |

**Table 2:** Existing resource consents

## 1.4 Proposal

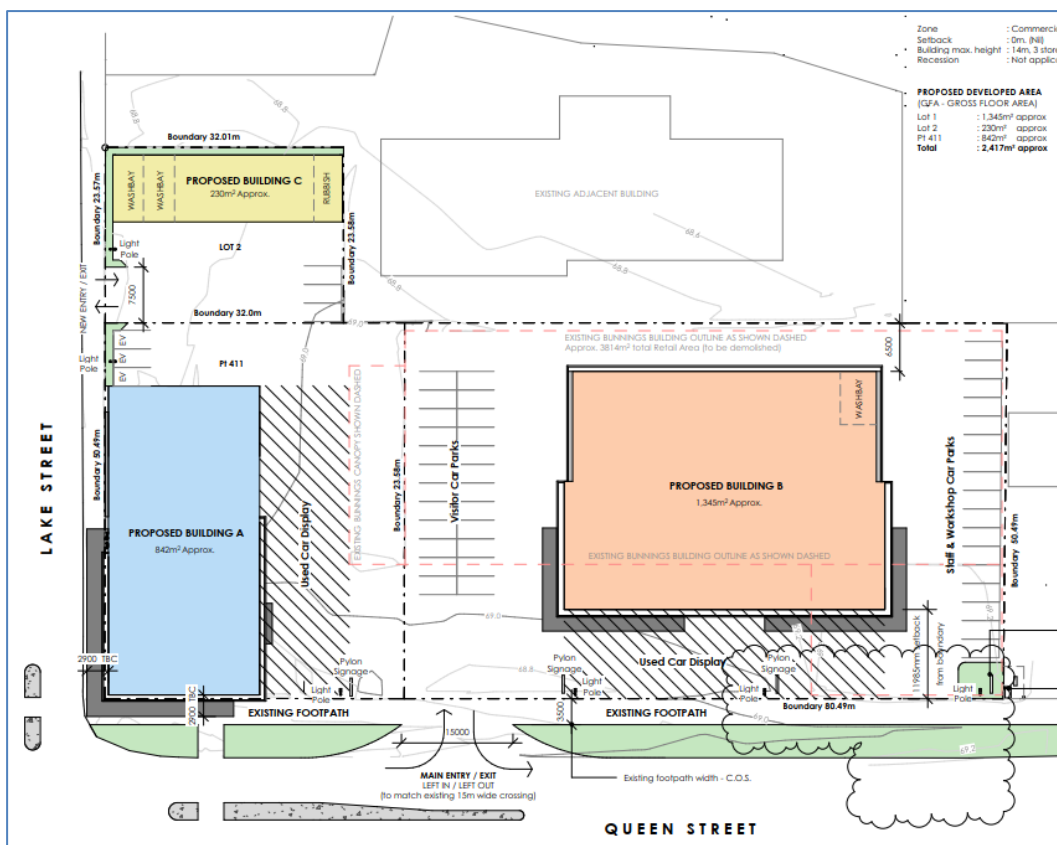
Pursuant to Section 88 of the Resource Management Act 1991 ('the Act') the Ingham Motor Group have applied for a land use consent to develop the site for a commercial car dealership and associated workshops for the servicing of vehicles.

### Built Form

The proposal includes three standalone single storey buildings and associated car parking for staff and visitors and the display of vehicles. The use includes car sales and associated car servicing and the intent is for the buildings to be used/occupied for multiple car dealerships however the land is to remain under single ownership and be used as a single entity.

Refer to Figure 5 for the proposed site plan for the activity.

Figure 5 Proposed Site Plan



Building A is setback 500mm from the western boundary and 500mm from the southern boundary. The southern section of the building contains a showroom and the northern section of the building contains a workshop. The building also contains an amenities area and a mezzanine level. Building A has a gross floor area of 842m<sup>2</sup>. The building has an overall height of 7.675m.

Building B is setback 11.985m from the southern boundary and 6.5 metres from the northern boundary. Building B contains a showroom which faces Queen Street, an amenities area, a workshop

and a wash bay. It contains multiple vehicle access points and roller doors. The building has an overall height of 7m.

Building C is setback 1m from the northern boundary and 1m from the western boundary. It includes internal wash bays, car grooming bays, rubbish bays and a miscellaneous storage room. The building has an overall height of 6m. This building will not be used for car sales. Refer to **Appendix A** for a full set of plans.

Building materials include; precast cladding (textured), glazed sliding entry doors and aluminium composite cladding and canopy, textured precast panels, exposed aggregate exterior slab and steps, bagged brick cladding (Building C only), aluminium composite with feature exterior window shroud, glazed façades, exposed aggregate steps, recessed entry portals and reverse run aluminium cladding. Refer to **Appendix B** for a set of elevations.

## Use

The proposed use will operate 7 days a week and include the following hours of operation; Monday to Friday 7.30am – 5.30pm. On the weekend the workshop will operate Saturday 7.30am – 1pm and will be closed on Sundays. The car sales will operate Saturday and Sunday 9am-4pm.

The proposal includes a maximum of 35 full time staff members. Visitor numbers are expected to range from 50 to 80 customers a day.

## Carparking/Access and Egress

The proposal will utilise two existing vehicle entry and exit points, one from Lake Street and one from Queen Street. The Lake Street entry/exit has a width of 7.5m and the main entry on Queen Street has a width of 15m.

The proposal includes used car display areas for Buildings A and Building B, visitor car parking and staff and workshop car parking and a loading bay.

The proposal includes 47 on site spaces for staff and visitors. A further 16 indoor car parking spaces will also be made available within the workshop areas, that can be used for car storage. On street carparking is also available along Lake Street and Queen Street. Two of the 47 on site spaces have been set aside for accessible parking. The proposed parking space dimensions meet AS/NZS 2890.1:2004 standards for off-street parking.

On site loading is proposed in a dedicated loading bay and will be conducted by a carrier truck that would be less than the standard 20m long carrier, therefore being able to be safely unload on site.

According to the BCD Group ITA Report, net trip generation associated with the previous Bunnings Warehouse included peak hourly trips of 95-137vph. The proposed car dealership and showroom has an estimated peak hourly trip rate of 48-97vph.



## **Stormwater**

The proposed stormwater management plan includes partial collection of runoff from the highly impermeable area into a piped network which will discharge directly to the council network, at approximately 187 l/s for the 10ys ARI storm. The proposal has included a Water Impact Assessment that has considered the whole site. The proposal utilises ground soakage for the two larger lots (lot 1 and Pt411) and utilises the existing connection to solely convey lot 2 into the council network.

## **Signage**

Two types of business identification signage are proposed which include pylon signage and wall mounted signage for Buildings A and B. The total area of wall mounted business identification signage is 33.6m<sup>2</sup>. The total area of pylon signage is 30m<sup>2</sup>. All signage is illuminated. The pylons will be on a daylight sensor, lux levels will not exceed 8 lux. Signage will not include flashing or moving elements.

## **Landscaping**

Planting is proposed along the western and northern edge of the property to enhance the Lake Street interface and the interface with the public space to the north. A number of climbing plants are also proposed for the western elevation of Building A and C. Planting is also proposed around the base of the pylon signage along Queen Street (in the south eastern corner of the site). Robinia Mop Tops are proposed within landscape beds along with selected understory planting.

## **Fencing**

The proposal includes retractable bollards of 500mm for the proposed entryways and two different fence types. Fence type 1 has a height of 1.2m and fence type 2 has a height of 1.6m. Both fence types have gaps of 25mm between palings.

## **Hardstand areas**

Hardstand vehicle parking areas and accessways include alternating textures in the design as well as large sections of asphalt. Pavers are included to soften the asphalt and add an aesthetic look to the site. A herringbone paving pattern is proposed with accents of orange and white punctuated with a black solid course that aligns with proposed fenestration of the buildings. Customer parking and display vehicle parking will be clearly differentiated by the use of asphalt or herringbone paving patterns.

## **1.5 Process Matters**

The proposal originally requested the inclusion of a new south eastern vehicle crossing. Issues with this crossing were originally raised in Council's Request for Further Information Letter dated 9 March 2023. The request stated that the south eastern vehicle entrance was not supported due to safety concerns and that the existing crossings should be used instead. The issue of separation distance from the existing junction was also raised and it was recommended that the crossing be removed.

Upon receipt of amended plans showing a left out scenario only for the south eastern crossing, BBO Traffic prepared a memo that came to the conclusion that additional information was required and the safety impacts were still relevant.

A further follow up email from BBO Traffic Dated 3 May 2023 confirmed that the initial issues were not addressed. It was BBO’s position that the need for the proposed south eastern crossing to a major arterial road corridor was not justified, and it created unnecessary safety risks for pedestrians and cyclists and negligible operational benefit for the public road network given the low number of movements.

Subsequently the applicant has removed the proposed south eastern crossing and plans to utilise the two existing crossings for the development. A new set of site plans were submitted on the 4 May 2023 showing the crossing removed.

**2 REASON FOR THE APPLICATION**

A land use consent as described under Section 87A of the Act is required for the reasons set out below.

**2.1 Resource Management (National Environmental Standard for Assessing and Managing Contaminants in Soil to Protect Human Health) Regulations 2011 (NES)**

These regulations came into force on 1 January 2012 and apply when a person wants to do an activity described in Regulation 5(2) to 5(6) on a piece of land described in Regulation 5(7) or 5(8). Following a review of the historical aerial photographs contained within Council’s records, a HAIL activity does appear to have been undertaken on the site. In accordance with Regulation 5(7), the site is a ‘piece of land’ and consent is required under the Resource Management (National Environmental Standard for Assessing and Managing Contaminants in Soil to Protect Human Health) Regulations 2011. A Preliminary Site Investigation (PSI) and a Contaminated Site Management Plan (CSMP) have been provided along with the application.

**2.2 Waipa District Plan Rule Assessment**

An assessment of the proposal’s compliance with the relevant rules of the District Plan has been completed. In summary, Table 3 below outlines the relevant rules relating to the proposed activity.

| Rule #      | Rule Name  | Status of Activity                | Comment  |
|-------------|--|-----------------------------------|--|
| 6.4.1.2 (c) | The erection of new buildings within the Cambridge B Character | Restricted Discretionary Activity | The proposed activity will result in the erection of three new buildings within the Cambridge B Character Precinct Area. |

|   | Precinct Area.                                       |                                   |   |
|---|--|-----------------------------------|---|
| 6.4.1.4 (d)   | Non-complying Activity                               | Non-complying activity            | All other activities not included in activity status table Rules 6.4.1.1 to 6.4.1.4. Motor Vehicle Sales is not included in the beforementioned rules.  |
| 6.4.2.0   | Pedestrian Frontage                                  | Discretionary Activity            | The buildings are not sited on the road boundaries as required by the rule and are setback from the road boundaries.  |
| 6.4.2.13  | Pedestrian Frontage, Max floor space                 | Discretionary Activity            | Combined floor area exceeds the 1,000m <sup>2</sup> GFA required in the rule.   |
| 6.4.2.21  | Identified Pedestrian Frontages                      | Discretionary Activity            | Verandahs are not continuous and do not meet the rule.  |
| 6.4.2.28  | Signs  | Discretionary Activity            | Signs are internally illuminated and exceed the number required by the rule   |
| 16.4.1  | Provision of car parks on site                       | Restricted Discretionary Activity | Provision of 25 or more carparks on site where 47 are proposed is a Restricted Discretionary Activity   |
| 16.4.2.22   | Provision of an Integrated Transportation Assessment | Required                          | Attached  |
| 22.1.1 (l)  | Heritage and Archaeology                             | Discretionary Activity.           | Proposal is located opposite the Cambridge Town Hall which is identified Category A Heritage Item (H35) and therefore requires consent.   |
| NES for Assessing and Managing Contaminants in Soil to Protect Human Health |  | Restricted Discretionary Activity | Site is considered a piece of land and no DSI has been prepared or presented. The proposal will most likely result in soil disturbance that exceeds the permitted threshold in Regulation 8 and is considered to therefore be a discretionary activity. |

**Table 3:** District Plan rule assessment

As outlined in the table above, the application is deemed to be a Non-Complying Activity being the highest status indicated by the above rules.

## 3 STAFF COMMENTS

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### 3.1 Environmental Health

Council's Environmental Health Officer, Glynn has reviewed the proposal and prepared conditions that have been included in this recommendation.

### 3.2 Development Engineering

Council's Development Engineer, Jonathan Marteja, has reviewed the application and has requested a number of conditions that have been included in this recommendation.

## 4 SECTION 104

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A decision was made under Section 95A and 95B of the Act to process the application on a non-notified basis. The Notification Decision was made by an independent hearing commissioner on the 6 June 2023. The independent hearing commissioner was Phil Mitchell and for the reasons outlined in his written decision, the application is to be processed on a non-notified basis. An assessment of and recommendation on the application pursuant to section 104 of the Act is set out below.

### 4.1 Section 104D - Threshold Test

Section 104D of the Act establishes a 'threshold test' or 'gateway test' that acts as an additional test for non-complying activities to satisfy. In order to pass the threshold test, a consent authority must be satisfied that the adverse effects of the activity on the environment will be minor **or** the activity will not be contrary to the objectives and policies of the District Plan.

The proposed activity is consistent with the relevant objectives and policies of the District Plan and the assessment of environmental effects in Section 4.5 of this report (in addition to the commissioners decisions dated 6 June 2023) demonstrates that any effects of the application will be **minor**. Therefore both limbs of the threshold test are accordingly met and as a consequence, the Council can have confidence that it has the necessary jurisdiction to determine the application in accordance with the statutory considerations contained within s104.

### 4.2 Section 104(1)A - Actual and Potential effects on the environment

#### 4.2.1 Effects Disregarded

Pursuant to Section 104(2), when forming an opinion for the purposes of Section 104(1)(a) a council may disregard an adverse effect of the activity on the environment if the plan or a NES permits an activity with that effect (i.e. a council may consider the "permitted baseline").

The "permitted baseline" and the "receiving environment" are discussed in Section 4.5.1 of the approved notification report (being a component of this combined report). This discussion and conclusion is also relevant for the purposes of the assessment under Section 104(1)(a). In summary,

it is concluded that the permitted activities under the zone and the existing Bunnings Warehouse built form and use forms part of the permitted baseline for this development. Therefore the effects of the existing Bunnings Warehouse and an array of commercial activities permitted by the District Plan can be disregarded.

Pursuant to Section 104(3)(a), when forming an opinion for the purposes of Section 104(1)(a), a council must not have regard to any effect on a person who has given written approval to the proposal, nor any trade competition or effects of trade competition. No written approvals have been provided with the application.

#### **4.2.2 Actual and Potential Effects**

The assessment of adverse effects in my notification recommendation are also relevant for the purposes of the assessment required under Section 104(1)(a). In summary it was concluded overall that some of the actual or potential adverse effects of the proposal **were** considered more than minor. The aspect of the design that was considered to have wider effect on the community was the lack of vibrancy the proposal would create in a wider strategic sense. However, on reflection and following the guidance of the commissioner set out in the notification decision, I have now changed my position in relation to wider community effects and consider that these will be no more than minor. Other effects such as; servicing, traffic, roading, carparking, signage, noise, heritage were considered no more than minor. However, the independent hearing commissioner's decision stated that the proposal does not warrant public or limited notification which concurrently indicates that the gateway test of Section 104D of the Act is satisfied.

Any effects resulting from the proposal were not considered worthy of notification. The notification decision goes as far as stating the *'proposal will have positive amenity and streetscape effects'*.

Once the 'gateway test' is past (which I consider it is) a greater gamut of assessment is afforded which ultimately concludes that the the actual and potential effects of the proposal are acceptable and can be avoided, remedied or mitigated through the imposition of conditions.

### **4.3 Section 104(1)(b) – Relevant Provisions**

#### **4.3.1 National Directions**

The application is not considered to be contrary to any National Directions that require consideration under the Resource Management Act.

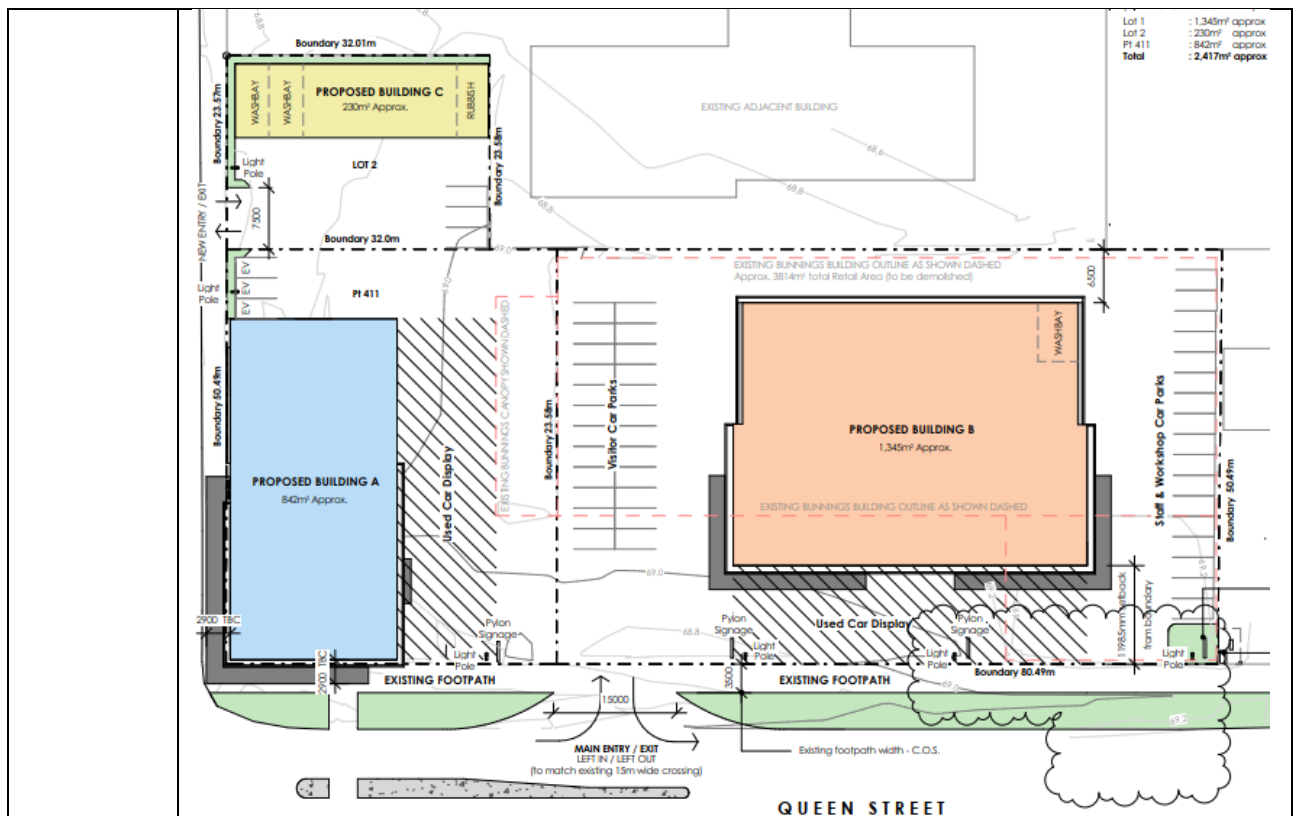
#### **4.3.2 Waikato Regional Policy Statement and Regional Plan**

The proposal is consistent with the relevant provisions of the Waikato Regional Policy Statement and the proposed land use will not affect any of the relevant provisions of the Regional Plan.

### 4.3.3 Waipa District Plan Objectives and Policies Assessment

The applicant’s agent has provided an assessment of the relevant objectives and policies. I have reviewed this assessment and the justification put forth. In addition, I have provided additional policy and objective and general assessment below:

|                          |  |
|--------------------------|--|
| Existing Site Conditions | <p>The subject site is 6,832m<sup>2</sup> in area and consists of three adjoining land titles. The site contains a vacant warehouse building which previously housed a Bunnings franchise. The existing building occupies 3,040m<sup>2</sup> of the subject site. The ex-Bunnings building has an overall height of 12 metres to the apex of the roof.</p> <p>The remainder of site (with the exclusion of landscape garden beds) is hard stand consisting of asphalt and concrete driveways. Landscape beds contain mature tree planting. The existing building is setback 18m from the southern boundary and provides car parking around the outer perimeter. The existing building has a poor interface with the streetscape and presents a number of poor design outcomes.</p>   |
| Existing context         | <p>Surrounding properties are commercial in nature. The land to the west contains take away food and automotive repair businesses. To the south of the subject site is a building constructed in 2016 containing a furniture retailer and homewares. To the southeast of the site a Mobil service station and a Countdown present hard paving and car parking to the streetscape. To the southwest of the site is the Cambridge Town Hall constructed in 1909. The hall is currently used for functions and events and is surrounding by public open space. West of the site is a Nissan car dealership and a McDonald’s restaurant. North west of the site contains a mixed use commercial and residential development and cinema. North of the site is car parking and a small grassed area with heritage value due to a small curved gate known as the ‘kissing gate’. Further north of the subject site is Lake Te Ko Utu and associated walking paths and vegetation reserve.</p> |
| Proposal                 | <p>Building A has an area of 842m<sup>2</sup> and is sited in the south west corner of the site. Building A is setback 500mm from the southern boundary and 500mm from the western boundary. A 2.9m wide canopy spans the Lake Street and Queen street footpaths. Building A presents glazing and contemporary building materials to both street frontages. The building proposes clerestory windows and planter beds and climbing plants.</p> <p>Building B has an area of 1,345m<sup>2</sup> and is setback 11.98m from the southern boundary. Building B presents its showroom to Queen Street and presents mainly glazing. It is contemporary in nature and has an overall height of 7m. Building C has an area of 230m<sup>2</sup> and is setback 1m from the northern and 1m from the western boundaries. The building uses contemporary building materials and uses landscaping to soften its appearance. A full set of drawings has been included in <b>Appendix 1</b>.</p>    |



The proposal is contemporary and includes the following building materials;

- Precast cladding;
- Glazed sliding entry doors and aluminium composite cladding and canopies;
- Textured precast panels;
- Exposed aggregate;
- Bagged brick cladding;
- Aluminium exterior feature walls and window shrouds;
- Herringbone paving pattern and asphalt;
- Landscape beds and climbing plants and
- Aluminium fencing.

Three illuminated pylon signs are proposed along the southern boundary. Wall mounted illuminated business identification signage is proposed for Building A and Building B. Six light poles are proposed along the southern and western boundaries.

The proposal includes allocated visitor car parks and areas set aside for used car displays. Coloured pavers laid in a herringbone configuration and asphalt will break up the hardstand areas.

Scale and Height

The height of the proposed design is consistent with commercial development along the streetscape. The proposed height of approx. 7m is consistent with the surrounding heights of the following businesses;

- Nissan;
- McDonalds;
- Subway/KFC;

|   |   |
|---|---|
|   | <ul style="list-style-type: none"> <li>• Contemporary Classics and Kiwi Home Store;</li> <li>• Mobil Service Station;</li> <li>• Countdown and</li> <li>• Z service station</li> </ul> <p>At 7m the proposal will not be at odds with the existing streetscape character. A flat roof design also reduces overall height and will be lower than the existing ex Bunnings building which sits at 12 metres.</p>  |
| Cambridge Town Hall - Heritage Building | Construction of new buildings on a site that adjoins a Category A listed heritage item in Appendix N1, where the buildings(s) is within 20m of the common boundary would warrant consent. The width of Queen Street is 30m and an assessment of the proposal against the Category B heritage item is not required by the ODP. However, the contemporary flat roof design, clean lines and chosen materials do not sit at odds or draw attention away from the Town Hall building. Such a contrast in architectural styles ensures that the contemporary buildings reads unoffensive whilst not detracting from the architectural merit of the heritage building. The proposal does not attempt to mimic or mock the heritage building instead presents a contrast by way of contemporary design and materials. The design is minimalist and does not include the level of detail found on the heritage building. The large setback between both buildings and the wide carriageway of Queen Street provide visual separation and creates two separate streetscape environs. |
| Design Response                         | The proposed buildings are single storey, well designed and articulated and use contemporary building materials resulting in the introduction of a contemporary commercial buildings into a tired commercial setting. The proposal complements the size, scale and height of built form found along Queen Street and Lake Street. The proposal replaces the unattractive ex-Bunnings building, associated hardstand areas and fencing and introduces well designed contemporary built form that is respectful to its context. The project will modernise and enhance the site and the surrounding area. Separation has been provided between the three buildings which reduces the visual impact of the buildings and creates a sense of connectivity through the site and as the site is highly permeable for pedestrians. The use of patterned paving to define and break up hard stand areas will provide visual interest and break up the large areas of car parking.   |
| Lighting                                | <p>Three illuminated pylon signs are proposed along the southern boundary. Wall mounted illuminated business identification signage is proposed for Building A and Building B. Six light poles are proposed along the southern and western boundaries.</p> <p>Illumination of commercial business identification signage is commonplace along Queen Street with large illumination associated with Mobil, Countdown, McDonalds, KFC and similar commercial businesses in the vicinity. There are no immediately adjoining residential properties that would be impacted by the proposed lighting and business identification signage. Consent conditions are to be included to control hours and illumination levels to ensure safety and appropriateness of illumination hours.</p>  |



| Objective   | Policy  |
|---|---|
| Objective 6.3.2<br>Vibrancy, design and character | Policy 6.3.2.1<br>To maintain the identified pedestrian frontages of the Commercial Zone as vibrant active places that support social and community well-being through building designs and activities that reinforce a pedestrian oriented retail environment.   |
|   | Policy 6.3.2.2<br>Outside of the identified pedestrian frontage enable larger scale retail activities, commercial service and vehicular orientated activities, where the activity does not impact on the role and function of the identified pedestrian frontages, including the vibrancy, amenity and social and community function of those areas.  |
|   | Policy 6.3.2.3<br>To ensure the incorporation of active street frontages in the design or redesign of buildings within the pedestrian frontage area by avoiding car parking and accessways, and through the provision of: (a) Passive surveillance; and (b) Verandahs.  |
|   | Policy 6.3.2.7<br>To maintain and where possible enhance the existing character of the character precinct areas in Cambridge, Te Awamutu, Kihikihi, Pirongia and Ōhaupo, by ensuring that new buildings; additions, or alterations to existing buildings, and signs make a positive contribution to the identified character of these areas, including by having architecturally detailed building frontages that incorporate appropriate designs and architectural features. |
|   | Policy 6.3.2.11<br>To ensure that buildings positively reinforce corner locations through building design, the position of the building on the site, architectural details, and by having prominent building entrances.   |

**Comment:** The subject site is located within a Pedestrian Frontage Area; areas identified in the District Plan to reinforce the pedestrian focus and vibrancy. Larger scale commercial activities are encouraged outside of these areas. Considering the existing environment and the forecasted future use of this area, the proposal is considered to add an element of vibrancy with Building A being located on the boundary whilst introducing a number of desired design elements and removing an unused derelict business.. On that basis, I consider that the proposal is an improvement on the existing environment.

The buildings are separated and designed in a manner to avoid the look of a single large scale building dominating the road frontage. The proposal is vehicle oriented however will not result in the visual impact associated with a car park for a use such as large format retailing or a supermarket. A number of design measures have been incorporated to mitigate the hard paving and car parking areas, the areas will be used for display and not for customer car parking (located elsewhere on the site). The urban design report describes the design measures for the hard stand

areas with an intent to draw from the building design and paving style found within the wider Cambridge. The Overview Plan shows a herringbone paving pattern with accents of orange and white punctuated by a black soldier course that aligns with the fenestration of the buildings. The pattern will emphasise the building entries with an increase in density and pattern detailing. Display areas will differ from customer parking areas and the remainder of the site is darker asphalt. The main visual areas of display will incorporate the proposed patterned paving.

The existing environment partially includes elements that lend to vibrancy, activation or pedestrian oriented retail, however the vast majority of properties along Queen Street do not present these qualities. The proposal enables an element of increased vibrancy by the siting of Building A in the southwest corner. It is considered unrealistic to expect the context to change significantly from the status quo, which comprises large format, vehicle dependent activities. These existing uses are established and likely to stay for considerable amounts of time (Countdown, Mobile, McDonalds etc).

Large areas of glazing, and large entry portico areas will ultimately activate the south west corner of the site and ensure a much better streetscape response than the existing Bunnings Building. Lake Street will be activated in addition to Queen Street. The proposal will provide passive surveillance, verandahs and will result in an increased level of amenity. It is considered that the building location, articulation and built form, the contemporary materials, landscaping and glazing will be an improvement from the existing environment.

Legal issues raised by Mr Lang in the notification hearing raised a number of relevant points of consideration in relation to the effects of the proposal and the existing environment. Mr Lang Stated:

- *The high traffic environment of Queen Street and isolation of the site from the rest of the town centre have prevented the site and its neighbouring sites from having high pedestrian activity. None of that is likely to change unless the function of Queen Street changes substantially. That is not a realistic outcome.*
- *Consideration of the future environment surrounding this site can be guided to a significant degree by the way in which the locality has developed in the past, the land title structure, in the context of the existing zoning and overlay.*
- *The District Plan provisions for Pedestrian Frontage Areas have been in place for approximately 9 years, without gaining any traction in this locality. That is understandable given the lot sizes, traffic environment, surrounding development and the ongoing opportunities for larger scale development surrounding the development site.*

The proposed design in the form proposed is considered to make a positive contribution to the identified character and introduces contemporary well designed architecture into a tired and aging streetscape. The proposal will meet the ultimate intent and purpose of the specific character areas it is located within and contribute positively to the environment.

| Rule   |  |
|--|--|
| Rule 6.4.2.1<br>Pedestrian frontages<br>building setback from<br>road boundaries | All new buildings within a pedestrian frontage area must be constructed on the road boundary of the site, provided that buildings on rear sites are exempt from this rule. |

The proposal does not technically comply with this rule however does place Building A 500mm from the Queen Street and Lake Street boundary. The fact that Building A is sited on a visually prominent corner and that the remaining built form on site complements Building A by way of contemporary architecture, the proposal is considered to sit comfortably within its context. The existing building is setback from all front and side boundaries and fails to provide any active frontage whatsoever. The introduction of Building A to the corner of the subject site and the introduction of a number of desired design outcomes such as prominent entries, glazing, canopy cover and street surveillance is considered to be a good outcome for the site and satisfy the above rule in this context.

| Rule  |   |
|---|---|
| Rule 6.4.2.13<br>Pedestrian frontages:<br>maximum floor space               | Within the pedestrian frontage area, any new building or activity located at ground floor level must not exceed 1,000m <sup>2</sup> GFA |
| Rule 6.4.2.14<br>Outside pedestrian<br>frontages: GFA retail<br>floor space | Outside the pedestrian frontage area any retail activity in a single tenancy or ownership must be greater than 1,000m <sup>2</sup>      |

Within the pedestrian frontage area, any new building or activity located at ground floor must not exceed 1,000m<sup>2</sup> GFA. The proposal has a total gross floor area of 2,414m<sup>2</sup>. The subject site has a total site area of 6,852m<sup>2</sup> which equates to a total site coverage of 35%. If you separate the buildings, Building B is the only building that exceeds the 1000m<sup>2</sup> requirement and due to low site coverage, very large building separation distances and the site being quite large, the built form response is considered to be an acceptable response within its context. The proposal will not present as large format bulky retailing and will sit comfortably within its context.

| Rule  |  |
|---|--|
| Rule 6.4.2.21<br>Identified pedestrian<br>frontages | Every new building, and additions/alterations to the front façade of existing buildings, which adjoins a pedestrian frontage area indicated on the Planning Maps shall: <ul style="list-style-type: none"> <li>(a) Provide transparent display windows extending over at least 75% of the area of the ground floor façade, comprising clear glass; and</li> <li>(b) Not have a vehicle crossing over the front boundary, provided that existing vehicle crossings may be retained; and</li> <li>(c) Provide a verandah designed in relation to its neighbours that:</li> </ul> |

|  |  |
|--|--|
|  | <ul style="list-style-type: none"> <li>(i) Provides a continuous verandah along the entire length of the property frontage that provides a waterproof connection to the verandah of the adjoining property; and</li> <li>(ii) Has a minimum depth of 3m and stops at least 0.5m short of the road kerb, except that where the distance between the building and road kerb is less than 3.5m, the verandah depth must be narrower to enable it to stop 0.5m short of the kerb; and Waipā District Plan Section 6 – Commercial Zone Page 18 of 24 Page Version – 27 January 2023</li> <li>(iii) Does not penetrate a height plane 4m parallel to the ground; and</li> <li>(iv) Has a minimum ground level stud height of 3m</li> </ul> |
|--|--|

The proposal complies in part with Rule 6.4.2.21. It does provide transparent glazing for part of the Queen Street and Lake Street frontages. It does provide a verandah, in part along the property frontage. Ideally the built form would have provided additional verandah coverage and have more of the built form located along the front boundary to activate the streetscape. However based upon the existing context and the future outcomes expected from this locality the proposal is considered to have positive amenity and streetscape outcomes. It will contribute positively to Queen and Lake Street.

#### **4.4 Section 104(1)(c) – Other Matters**

##### **4.4.1 Tai Tumu, Tai Pari, Tai Ao – Waikato Tainui Iwi Environmental Management Plan**

Tai Tumu, Tai Pan, Tai Ao purpose is to enhance collaborative participation between Waikato Tainui and agencies in resource and environmental management. It provides high level guidance on Waikato Tainui values, principles, knowledge and perspectives on, relationship with, and objectives for natural resources and the environment. The plan highlights the need for enhancement and protection of landscape and natural heritage values. Site management protocols ensure a precautionary approach to managing (known or undiscovered) wahi tapu sites and taonga tuku iho discovery. Adequate control of sediments and erosion prevention are target areas to improve water quality. Methods that Waikato Tainui support are the development of erosion and sediment control plans and input into monitoring of those plans (21.3.1.2 a) and b)).

The property is within the Waikato Tainui Joint Management Agreement Area and therefore the provisions of the Tai Tumu, Tai Pari, Tai Ao are relevant. However there is nothing in the application that will conflict with the desired outcomes of Tai Tumu, Tai Pan, Tai Ao and accordingly, I consider the proposal to be consistent with the plan.

#### **4.4.2 Te Rautaki Tamata Ao Turoa o Haua — Haua Environmental Management Plan**

Te Rautaki Tamata Ao Turoa o Haua explains the importance of communication between local authorities and Ngati Haua in terms of keeping the Iwi Trust informed about projects, providing a feedback loop and opportunity for relationship building. The plan clearly outlines that engagement is expected for earthworks activities and that the Iwi seek opportunities to participate in consent and site monitoring and restoration projects.

The property is within the Ngati Haua Area of Interest and therefore the provisions of Te Rautaki Tamata Ao Turoa o Haua are relevant. There is nothing in the application that will conflict with the desired outcomes of Te Rautaki Tamata Ao Turoa o Haua accordingly, I consider the proposal to be consistent with the plan.

#### **4.4.3 Te Rautaki Taiao a Raukawa — Raukawa Environmental Management Plan**

Te Rautaki Taiao a Raukawa provides a statement of Raukawa values, experiences, and aspirations pertaining to the use and management of the (their) environment. It is considered as a living and practical document that provides guidance on proactive, effective engagement in shaping current and future policy, planning processes, and resource management decisions.

The property is within the Raukawa Area of Interest and therefore the provisions of Te Rautaki Taiao a Raukawa are relevant. There is nothing in the application that will conflict with the desired outcomes of Te Rautaki Taiao a Raukawa accordingly, I consider the proposal to be consistent with the plan.

#### **4.4.4 Treaty Settlement Acts – Areas of Interest (AOI)**

The property subject to this consent is within Ngati Haua and Ruakawa Deed of Recognition Area, and within Ngati Haua and Raukawa Areas of Interest.

The applicant has stated that they have had correspondence with Dr Warren Gumbley who notes that given the recent history of the area and the development of the Bunnings building, that the development of the Bunnings Warehouse will have destroyed any archaeology that may have been present on the site. The applicant states that Dr Warren Gumbley has conferred with Rachel Darmody of Heritage NZ, and they have confirmed that no Heritage Authority is required.

Therefore for this reason, the proposal is considered to not be contrary to the cultural, spiritual, historical, and traditional association of Ngāti Hauā and Raukawa with this identified area.

#### **4.4.5 Council Bylaws**

The following Council bylaws have been reviewed and considered with regard to the proposed development:

- Stormwater Bylaw 2019;
- Wastewater Drainage Bylaw 2011; and

- Water Supply Bylaw 2013.

The Stormwater Bylaw 2019 outlines the requirements with regard to open drains existing in the Waipa District which are important components in the stormwater disposal system. The ‘Protection of Land Drainage systems’ of this Bylaw sets out controls designed to ensure that these open drains are not obstructed or restricted in any way. The stormwater section of the Bylaw applies to stormwater drainage from both domestic and trade premises connected to the Waipa District Council public stormwater drain. As the proposal seeks to discharge to the open stormwater drain, an approval is required under this Council Bylaw.

The Wastewater Drainage Bylaw 2011 outlines requirements with regard to wastewater drainage from both domestic and trade premises to the Council’s wastewater system. As the proposal does seek to connect to Council’s wastewater reticulation network, an approval or authority is required under this Council Bylaw.

The Water Supply Bylaw 2013 applies to any person being supplied with, or who has made an application to be supplied with, water by the Council, and promotes the efficient use of water and protecting against waste or misuse of water from the water supply system and the management and protection of infrastructure associated with the water supply network. Under this Bylaw a written application is required to Council in order to establish new water connections. Council’s Water Services Department have confirmed connection from Lot 2 via email on the 15<sup>th</sup> June 2023 and will request a proposed/concept plan as a result of an approval.

#### **4.4.6 Development Contributions**

The proposal has been assessed against Council’s Development Contribution Policy 2022. In accordance with Section 197 of the Local Government Act 2002, development contributions can only be required where a development is defined as follows:

- “(a) Any subdivision or other development that generates a demand for reserves, network infrastructure, or community infrastructure; but*
- (b) Does not include the pipes or lines of a network utility operator.”*

A Development Contribution Notice will be issued with the decision of this consent if it is granted.

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## 5 RECOMMENDATION

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The above assessment has concluded that any actual and potential effects of the proposal are acceptable and the proposal is considered to satisfy the relevant objectives and policies of the District Plan as well as being consistent with the Waikato Regional Policy Statement and all other relevant matters.

The proposed activity meets the purpose and principles of Part 2 of the Act and therefore subject to the conditions listed within the decision to be served under Section 113, the proposal can be granted under the District Plan.

**Reporting Officer:**



**Paul Iacuone**  
**Planner**

**Dated: 29/6/2023**

**Reviewed for Release By:**



**Quentin Budd**  
**Consents Team Leader**

**Dated: 4 July 2023**

**DECISION ON APPLICATION: LU/0023/23**

Pursuant to Sections 34A(1), 104, 104B and 108 of the Resource Management Act 1991, the Waipa District Council, under delegated authority, grants Land Use Consent for a Non-complying Activity to:

**Activity:** Use and development of a Non-complying Activity including a car dealership, showroom and workshop within the Cambridge B Character Precinct Area dispensing with various Commercial Zone performance standards.

**Consent Holder: Ingham Motor Group**

**Location Address: 26 Lake Street, Cambridge 3434**

**Legal Description:** Lot 1 DP 56389 as comprised in Record of Title SA46B/898  
Lot 2 DP 56389 as comprised in Record of Title SA46B/899  
Southern Allotment 411 Town of Cambridge East as comprised in  
Record of Title SA48/249

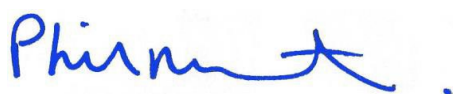
This consent is subject to the conditions attached in Schedule 1.

Advisory notes for this consent are attached in Schedule 2.

The reasons for this decision are attached in Schedule 3.

Dated at Cambridge this 6<sup>th</sup> day of July 2023.

For and on behalf of Waipa District Council.



Phil Mitchell  
**INDEPENDENT COMMISSIONER**



# Schedule 1

## Conditions of Consent

Resource Consent No: LU/0023/23

### General

- 1 The proposal must proceed in general accordance with the information submitted with the application dated 21 February 2023 and Site Plan SK1.05 dated 4 May 2023, except where another condition of this consent must be complied with. This information is entered into council records as LU/0023/23. A copy of the approved plans are attached.

### Monitoring

- 2 The consent holder must notify the Waipa District Council enforcement team in writing two weeks prior to the commencement of activities associated with this consent.

**Note:** This advice should be emailed to:- [consentmonitoring@waipadc.govt.nz](mailto:consentmonitoring@waipadc.govt.nz).

### Engineering

3. No less than 10 working days prior to construction activities commencing on site, the consent holder shall provide a finalised Construction Management Plan to Council's Team Leader – Development Engineering for certification. The plan shall include, but is not limited to:
  - a) Site management arrangements
  - b) Proposed construction program
  - c) Construction methodologies
  - d) Access, Manoeuvring and parking arrangements
  - e) Hazardous substance management
  - f) Final detailed engineered design drawings
  - g) Sediment control / Dust Control / Noise control
4. The consent holder shall submit design/construction plans for the proposed carpark in general accordance with the Proposed Site Plan, DWG No. SK1.05 (dated 4 May 2023). The Design/Construction plans shall be submitted to Council for certification prior to carrying out any construction work required by this consent. All work associated with the carpark shall be designed, constructed and completed to the acceptance of the Council's Team Leader – Development Engineering and at the consent holders expense. The submitted plans shall include, but is not limited to:
  - a) Pavement design – based on testing of existing ground;
  - b) Tracking curve analysis and parking line delineation dimensions
  - c) Test results of in-situ ground for the portion of new pavement to be constructed

- d) Disposal of stormwater
  - e) Common services trench
  - f) Surface treatment
  - g) onsite lighting
5. Following completion of the carpark areas required under condition 4, Quality Assurance Certificates from a suitability qualified and experienced professional shall be completed, signed and submitted to Council's Team Leader – Development Engineering for certification.
  6. All parking spaces within the parking area as shown on the approved site plan shall be marked or delineated on site *at the consent holders expense*.
  7. Prior to Code of Compliance Certificate for any of the buildings, the consent holder shall upgrade as required the existing vehicle crossing **at Lake Street and Queen St**. All work shall be carried out and completed to the acceptance of Council's Team Leader – Development Engineering, and shall be at the consent holder's expense. The following issues shall also be addressed:
    - a) A vehicle crossing application will need to be completed.
  8. The consent holder shall arrange for Council to install separate water connections to Lot 1 , lot 2 and at PT114 at the consent holder's expense.
  9. As-built plans and information of all water infrastructure shall be provided to the acceptance of Council's Team Leader – Development Engineering and shall be at the consent holder's expense.
  10. The consent holder shall submit design/construction plans for the gravity wastewater reticulation system to supply the proposed lots and existing receiving network shown in general accordance with the Proposed Site Plan Three Waters , DWG No. RC2 Rev.1. The design/construction plans shall be submitted to Council for acceptance prior to carrying out any construction work required by this consent. This system shall be designed to the acceptance of Council's Team Leader – Development Engineering, and shall be at the consent holder's expense. The submitted plans shall include, but is not limited to:
    - a) Flow direction and grades;
    - b) Pipe sizing and material;
    - c) Bedding details;
    - d) Manhole sizing and details;
    - e) Longitudinal sections;
    - f) Connections to service Lots 1, 2 and PT114
  11. The consent holder shall construct wastewater gravity reticulation prior to Code of Compliance Certificate for the respective building as per the approved design/construction submitted under **Submit gravity wastewater reticulation design** Condition 10 above and to

the acceptance of Council's Team Leader – Development Engineering at the consent holder's expense.

12. Following completion of the wastewater gravity reticulation required under Condition **Construct gravity reticulation** above, Quality Assurance Certificates from a suitably qualified and experienced professional shall be completed, signed and submitted to Council's Team Leader – Development Engineering within 10 working days of works being completed for acceptance.
13. As-built plans and information of all wastewater infrastructure shall be provided to Council's Team Leader – Development Engineering for certification within 10 working days of works being completed and shall be at the consent holder's expense.
14. The consent holder shall submit Design/construction plans for the stormwater reticulation system to supply the proposed lots and existing receiving network in general accordance with the Proposed Site Plan Three Waters, DWG No. RC2 Rev.1 The Design/Construction plans shall be submitted to Council for certification prior to carrying out any construction work required by this consent. This system shall be designed to the acceptance of Council's Team Leader – Development Engineering, and shall be at the consent holder's expense. The submitted plans shall include, but is not limited to:
  - a) Flow direction and grades;
  - b) Pipe sizing and material;
  - c) Longitudinal sections;
  - d) Overland flow paths;
  - e) Receiving network outlet details;
  - f) Bedding details;
  - g) Manhole sizing and details;
15. The consent holder shall construct the stormwater reticulation as per the certified design/construction plan under Condition 14 – Stormwater - Construct above prior to Code of Compliance Certificate for the respective building and to the acceptance of Council's Team Leader – Development Engineering at the consent holder's expense.
16. Following completion of the stormwater reticulation required under Condition 14 - Stormwater - **Construct** above, Quality Assurance Certificates from a suitably qualified and experienced professional shall be completed, signed and submitted for certification to Council's Team Leader – Development Engineering within 10 working days of works being completed.
17. As-built plans and information of all stormwater infrastructure shall be provided for the certification of Council's Team Leader – Development Engineering within 10 working days of works being completed and shall be at the consent holder's expense.

## **Construction Noise**

18. All construction noise must be measured and assessed in accordance with New Zealand Standard NZS 6803:1999 Acoustics – Construction Noise.
19. For construction works including concrete pouring, the time periods and maximum noise levels must not exceed the upper limits for long duration works contained in Table 2 of New Zealand Standard 6803:1999 (Acoustics – Construction Noise).

## **Construction Noise and Vibration Management Plan**

20. The consent holder must submit a finalised Construction Noise and Vibration Management Plan (CNVMP) to Council's Environmental Health Team Leader for certification, at least two weeks before work commences on site. The CNVMP must include methodologies and mitigation measures to specifically control emissions from concrete breaking and if applicable, concrete crushing on site.
21. The Construction Noise and Vibration Management Plan (CNVMP) must be incorporated into the contractor's Construction Management Plan (CMP) and implemented at all times during construction works.

## **Dust**

22. That as a result of activities authorised by this resource consent, there must be no discharge of dust to air that causes an objectionable or offensive effect beyond the consented site boundary. If offensive or objectionable dust emissions do occur beyond the consented site boundaries, the dust-causing activity must cease immediately and must not recommence until appropriate measures have been put in place to prevent recurrence of a similar event.

## **Operational Lighting**

23. External lighting and associated fittings shall be designed, placed and controlled so that the height, colour, orientation and screening avoids light spill and glare onto any receiving sites.

## Schedule 2

### Advisory Notes

#### Resource Consent No: LU/0023/23

1. This consent is granted by the Council subject to the Council's officers and/or agents being permitted access to the property at all reasonable times for the purposes of carrying out inspections, surveys, investigations, tests, measurements or taking samples.
2. All earthworks associated with any development of land should be undertaken in accordance with the following matters:
  - a) Carried out so as to provide sound foundations as required under NZS 4431:1989 and avoid any hazard to persons or property;
  - b) Carried out so as to avoid or mitigate any detrimental effect on the environment particularly with regard to the unnecessary destruction of vegetation, the contamination of natural water or the diversion of surface or ground water flows;
  - c) The existing landform not be altered in such a manner that adjoining properties will be detrimentally affected particularly through changes in drainage systems or abrupt changes in ground level.
3. Building consent is required from Waipa District Council for the construction of the development.
4. Any work that is required to be carried out shall be in accordance with the Regional Infrastructure and Technical Specification (RITS) and shall be at the consent holders expense.
5. Proprietary cell systems offer a far greater option in terms of long term serviceability. They allow for easier ongoing maintenance where systems can be flushed, as opposed rock lined trenches that once filled will require full replacement.
4. Entrance construction and crossing standards are set out in the Regional Infrastructure and Technical Specification (RITS)
5. All entrance work within the road corridor shall only be carried out by a Waipa District Council approved Contractor. There is no additional application fees associated with this application.
6. All contractors or persons undertaking work in the road corridor, for which reinstatement work will be necessary, are required to make a Corridor Access Request (CAR) via the Submitica web site ([www.submitica.com](http://www.submitica.com)). A Traffic Management Plan for the works shall be submitted with the CAR.

7. PT114 may use the existing connection upon Engineer's confirmation that the existing supply is adequate. An administrative, and installation fee will apply to the application.
8. The Regional Infrastructure and Technical Specification (RITS) sets out a means of compliance for the design and construction of all stormwater infrastructure assets.
9. All private stormwater infrastructure is to comply with Waipa District Council's Stormwater Bylaw 2019;  
Section 7: Protection of Land Drainage Systems – Item 7.5; and  
Section 9: Private Stormwater Systems - All items.
10. Confirmation of Council's inspections will be made at the Pre-Construction Meetings. Council's Engineers require a minimum of 48 hours' notice prior to an inspection.
11. Draft As-built Plans and information of all infrastructure, shall be provided prior to the final inspection.

## Schedule 3

### Reasons for Decision

#### Resource Consent No: LU/0023/23

- 1 The proposal is not contrary to Section 5, 6 or 7 of the Act. There is nothing in the proposal that would conflict with the principles of the Treaty of Waitangi (Section 8 of the Act). Overall, the application would not offend any of the matters contained within Part 2 of the Act.
- 2 The proposal is a Non-complying Activity under the Waipa District Plan. The proposal will have no more than minor adverse effects on the environment and is not contrary to the relevant objectives and policies of the Waipa District Plan.
- 3 Any adverse environmental effects resulting from the proposal are deemed to be minor and can be mitigated by the imposition of appropriate consent conditions. The proposal is therefore considered to meet the gateways tests of Section 104D of the Resource Management Act, for Non-Complying Activities.
- 4 The application was processed on a non-notified basis and was approved via an independent hearing commissioner on the 6<sup>th</sup> June 2023. Written approval was not obtained from any party.
- 5 Compliance with conditions relating to monitoring will avoid unnecessary site inspections being made (and inspection fees charged) by Council's Monitoring and Enforcement team.
- 6 The accidental discovery protocol conditions are required to ensure the consent holder is aware of their obligations in regards to the discovery of taonga (treasured or prized possessions, including Māori artefacts), archaeological sites, or skeletal remains.

## DEVELOPMENT CONTRIBUTION NOTICE

DC No: DC/0038/23

Date Issued: 22/03/2023

To: Ingham Motor Group

Property: 26 Lake Street, Cambridge 3434

Details: LU/0023/23 Establish a car dealership

| Code     | Description                             | Housing<br>Equivalent Units | Rate (\$)     | Total               |
|----------|---|-----------------------------|---------------|---------------------|
| DC22RDCB | DC 2022 Roading and Transport Cambridge | 23.00                       | \$5170.00     | \$118910.00         |
| DC22WCB  | DC 2022 Water Supply Cambridge          | 7.50                        | \$1891.00     | \$14182.50          |
| DC22WWCB | DC 2022 Wastewater Cambridge            | 7.50                        | \$4177.00     | \$31327.50          |
|          |   |                             | <b>Total:</b> | <b>\$164,420.00</b> |

GST Inclusive

In accordance with section 208(a)(ii) of the Local Government Act 2002 and section 116(1) of the Resource Management Act 1991, the landuse consent must not commence until the development contribution of \$164420.00 is paid.

The development contribution must be paid to Waipa District Council at one of the addresses printed at the top of this notice.

If the development contribution is not paid the Council may, under section 208(d) of the Local Government Act 2002, register the development contribution under the Statutory Land Charges Registration Act 1928 as a charge on the title of the land in respect of which the development contribution was required for.

This Development Contribution Notice is not an invoice.



AUTHORISED OFFICER SIGNATURE:



In accordance with Council's Development Contribution Policy, this development requires a Development Contribution to be paid. This Development Contribution Notice provides details of the development contribution calculations and the total amount payable. More information is available on request from the Council.

The development contribution is calculated based on the published schedule of fees outlined in Council's Development Contribution Policy & Fees and Charges.

Further or subsequent development contributions may be required on the granting of any building consent, resource consent, or service connection granted.

Further information on the Development Contribution Policy is available on Council's website: [www.waipadc.govt.nz](http://www.waipadc.govt.nz) (go to Planning and Resource Consents).

### **Reconsiderations and objections**

If you are not happy with this assessed development contribution, you may apply to Council for a reconsideration of the assessment, or you may lodge a formal objection. A reconsideration is considered by Council staff, and a decision is issued to you within 15 working days of Council receiving all the information required to make the decision. A formal objection is heard by an Independent Development Contribution Commissioner appointed by the Council, and the majority of the costs associated with the objection process will be required to be paid by you. For further information on both processes please refer to the information sheet on Council's website: [www.waipadc.govt.nz](http://www.waipadc.govt.nz) (under planning and resource consents), email [DCenquiry@waipadc.govt.nz](mailto:DCenquiry@waipadc.govt.nz), or call Council on 0800 924 723.

### **Note:**

1. You must lodge any request for reconsideration with Council within 10 working days after the date on which you receive this notice.
2. You must lodge a formal objection with Council within 15 working days after the date on which you receive this notice, or within 15 working days after you receive Council's decision on an application for reconsideration (if you lodged an application for reconsideration).
3. Refer to Council's website for reconsideration and objection application forms.

**Development Contributions may be paid via internet banking to Waipa District Council - ASB - 123122-0084701-01.**

Please reference the application number and payer name to ensure payment is correctly allocated, and forward the remittance advice to [rates@waipadc.govt.nz](mailto:rates@waipadc.govt.nz)

**TOWN PLANNING INFORMATION:**

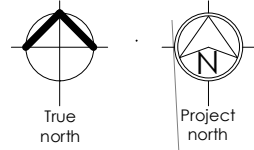
Zone : Commercial.  
 Setback : 0m. (Nil)  
 Building max. height : 14m, 3 storeys.  
 Recession : Not applicable.

**SITE INFORMATION:**

Address : 26 Lake Street, Cambridge.  
 (Corner of Lake and Queen Street).

Area (Lot 1) : 4075m<sup>2</sup>.  
 Area (Lot 2) : 754m<sup>2</sup>.  
 Area (Pt 411) : 2023m<sup>2</sup>.  
 Area (Total 3 sites) : 6852m<sup>2</sup>.

DP number : DPS 56389.  
 Climate Zone : 2.  
 Earthquake Zone : Zone 1.  
 Exposure Zone : Zone B.  
 Wind Zone : Medium.  
 WDC Zone : Commercial.

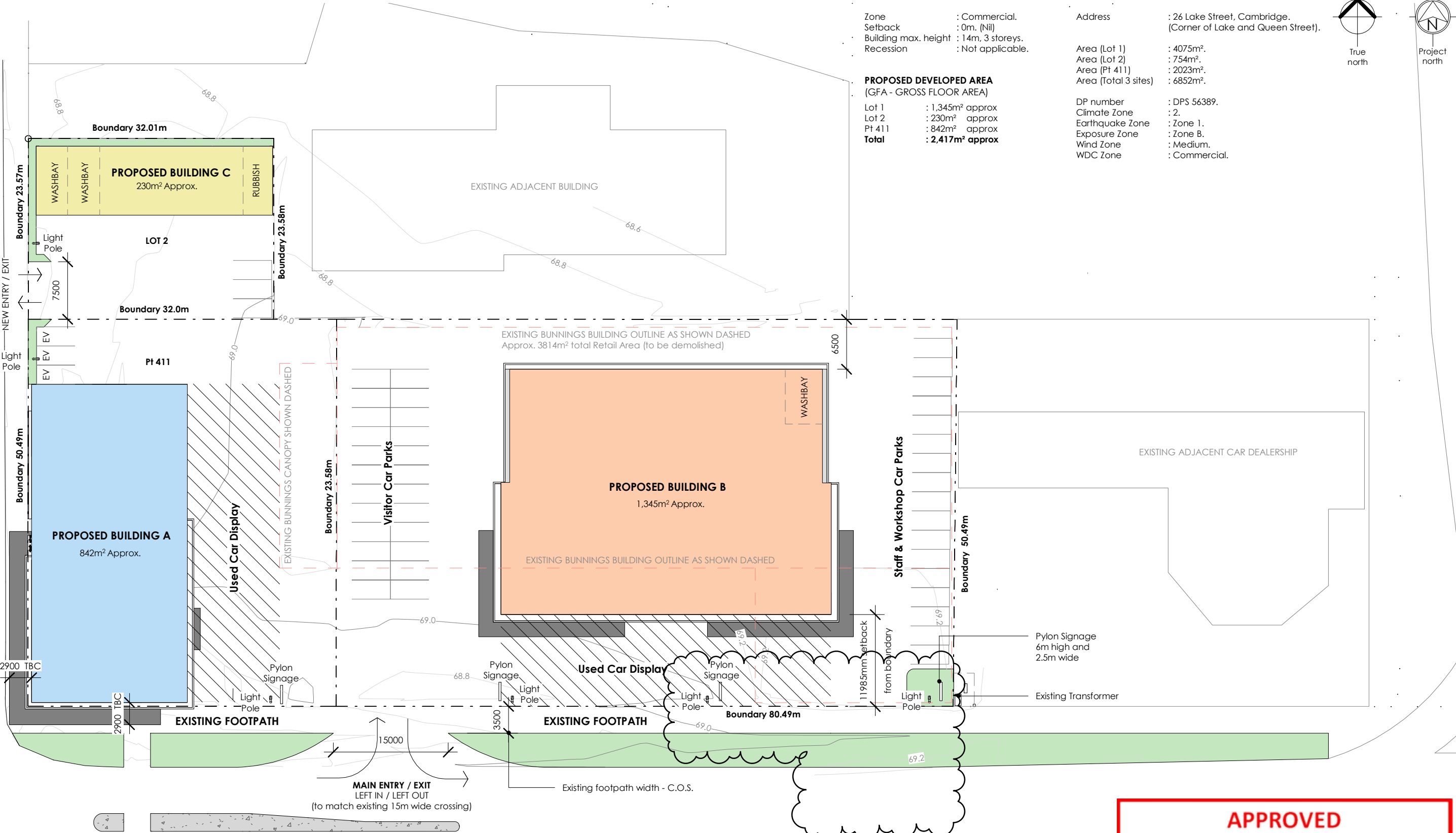


**PROPOSED DEVELOPED AREA**  
 (GFA - GROSS FLOOR AREA)

Lot 1 : 1,345m<sup>2</sup> approx  
 Lot 2 : 230m<sup>2</sup> approx  
 Pt 411 : 842m<sup>2</sup> approx  
**Total : 2,417m<sup>2</sup> approx**

LAKE STREET

QUEEN STREET

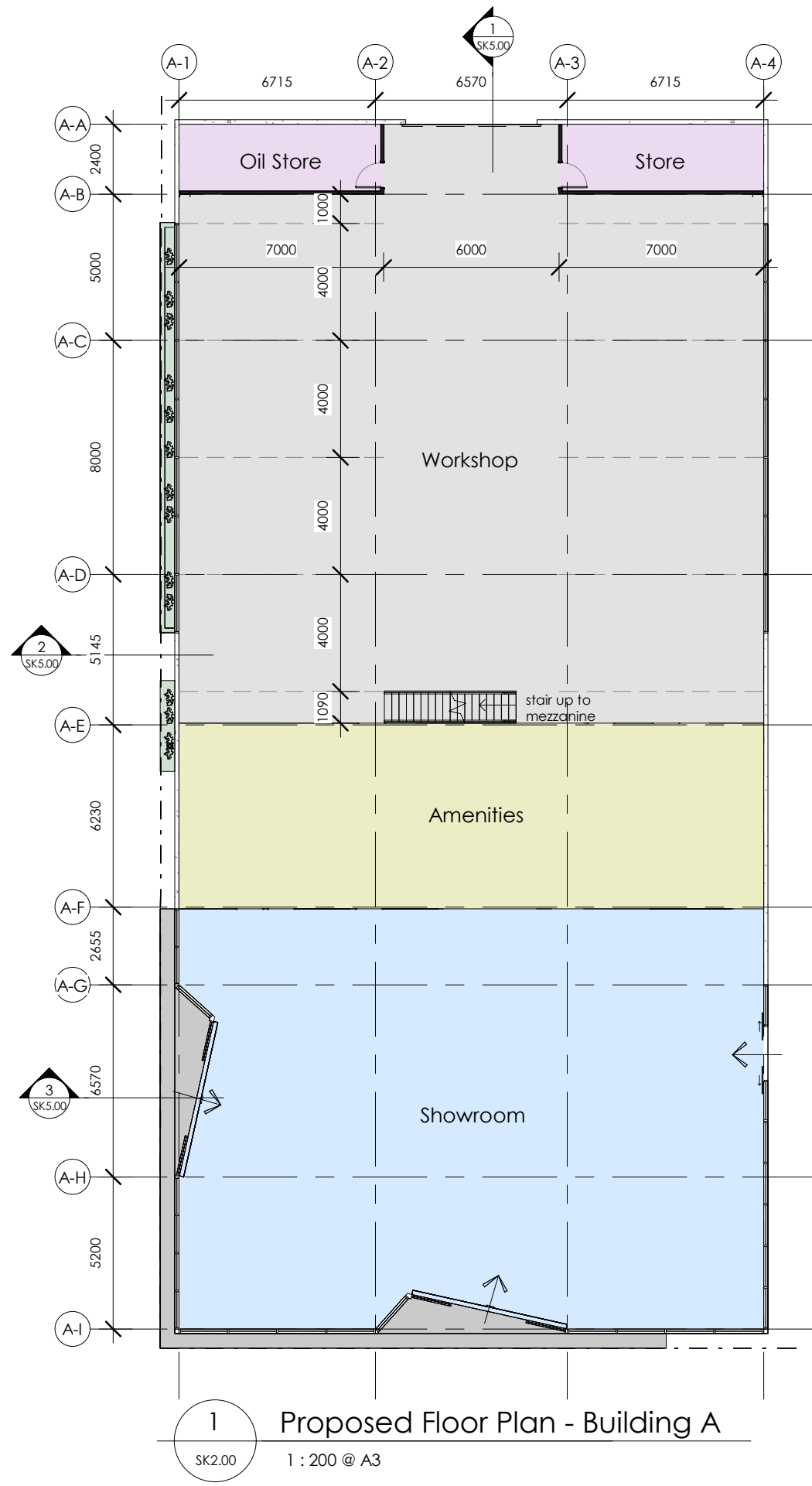
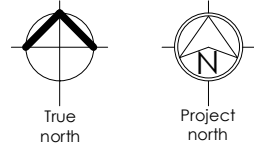


**APPROVED**  
**Waipa District Council**  
 Subject to the conditions of the  
 resource consent LU/0023/23

1 Proposed Site Plan  
 SK4.01 1 : 500 @ A3

| REV. | DATE     | REVISION DETAILS                           |
|------|----------|--|
| G    | 04.05.23 | Revised for RC                             |
| F    | 17.03.23 | RC RFI 01                                  |
| E    | 12.12.22 | Issued for Discussion                      |
| D    | 6.12.22  | Issued to Client for Review                |
| C    | 5.12.22  | DRAFT for Comment                          |
| B    | 13.10.22 | Concept Design - dimension clarifications. |
| A    | 05.09.22 | Concept Design                             |

|                             |                     |                |
|-----------------------------|---------------------|----------------|
| Scale:<br>As indicated @ A3 | Date:<br>04.05.2023 | Drawn:<br>HS   |
| Job No:<br>1561             | Designed:<br>DLA    | Checked:<br>MS |
| Drawing No:<br>SK1.05       | Revision No:<br>G   |                |



1 Proposed Floor Plan - Building A  
SK2.00 1 : 200 @ A3

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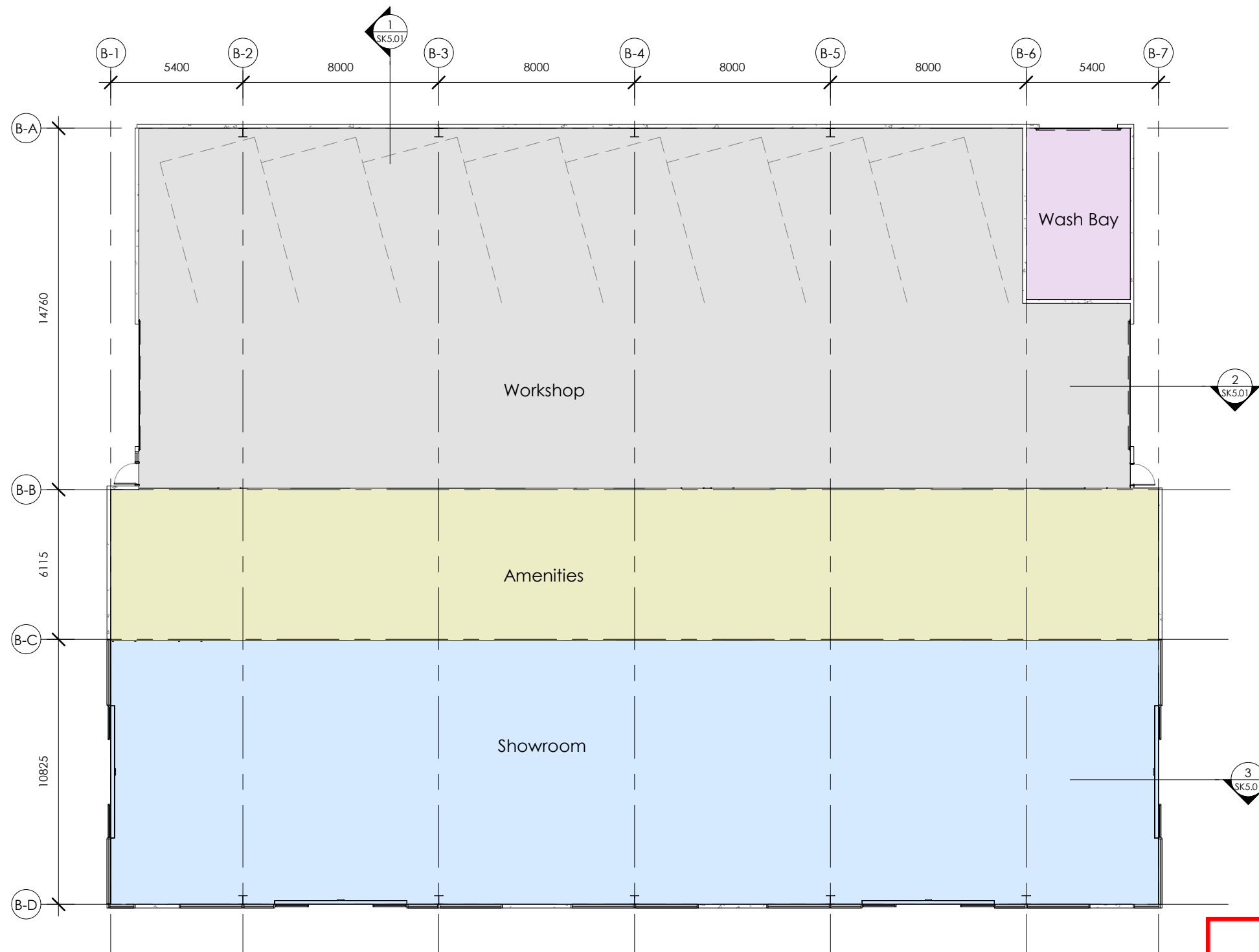
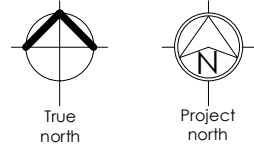
Project:  
New Car Yard Development  
26 Lake Steet, Cambridge.

Client:  
Ingham Driven

Drawing Title:  
Proposed Floor Plan - Building A  
CAD File: Autodesk Docs://561 New Car Yard Development/561 Ingham Cambridge New Development\_Master.rvt

| REV. | DATE     | REVISION DETAILS            |
|------|----------|-----------------------------|
| B    | 27.01.22 | Issued for Resource Consent |
| A    | 12.12.22 | Issued for Discussion       |

|                        |                     |                   |
|------------------------|---------------------|-------------------|
| Scale:<br>1 : 200 @ A3 | Date:<br>12.12.2022 | Drawn:<br>HS      |
| Job No:<br>1561        | Designed:<br>DLA    | Checked:<br>MS    |
| Drawing No:<br>SK2.01  | Revision No:<br>49  | Revision No:<br>B |



1 Proposed Floor Plan - Building B  
SK2.00 1 : 200 @ A3

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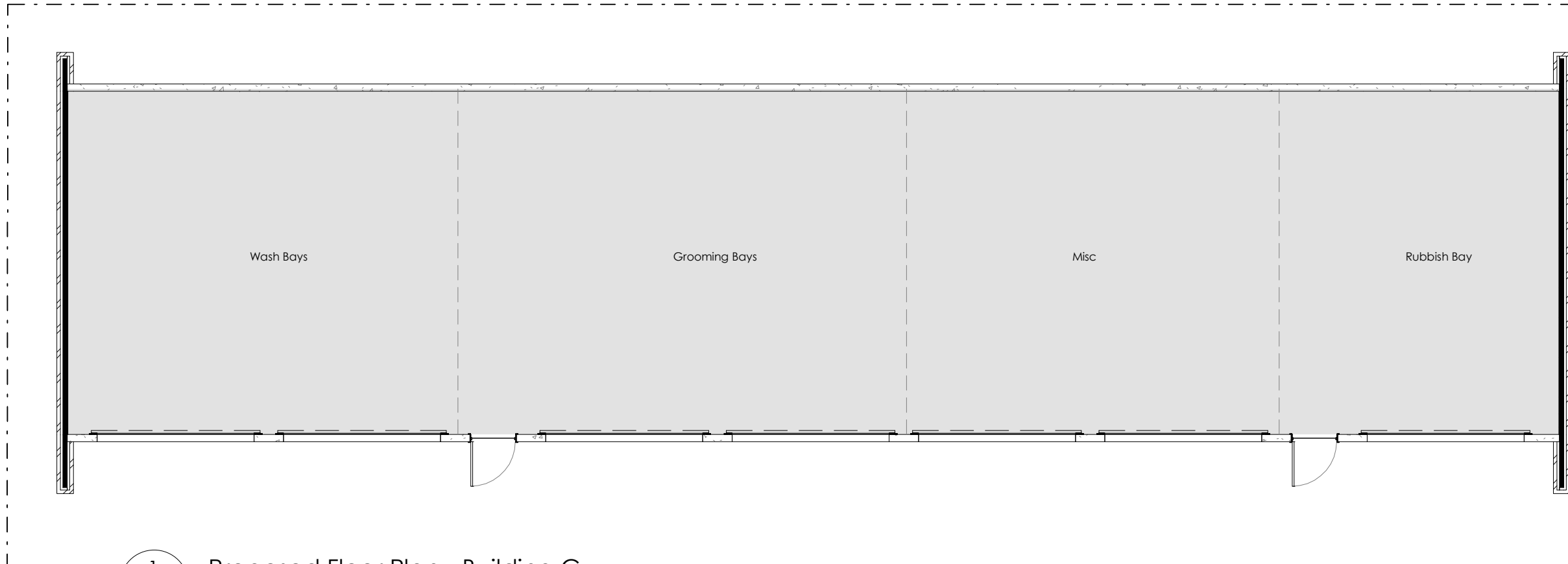
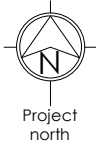
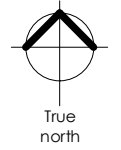
Project:  
**New Car Yard Development**  
26 Lake Steet, Cambridge.

Client:  
**Ingham Driven**

Drawing Title:  
**Proposed Floor Plan - Building B**  
CAD File: Autodesk Docs://1561 New Car Yard Development/1561 Ingham Cambridge New Development\_Master.rvt

| REV. | DATE     | REVISION DETAILS            |
|------|----------|-----------------------------|
| B    | 27.01.22 | Issued for Resource Consent |
| A    | 12.12.22 | Issued for Discussion       |

|                        |                     |                   |
|------------------------|---------------------|-------------------|
| Scale:<br>1 : 200 @ A3 | Date:<br>12.12.2022 | Drawn:<br>HS      |
| Job No:<br>1561        | Designed:<br>DLA    | Checked:<br>MS    |
| Drawing No:<br>SK2.02  | Revision No:<br>50  | Revision No:<br>B |



1 Proposed Floor Plan - Building C  
SK2.00 1 : 100 @ A3

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**resource consent LU/0023/23**

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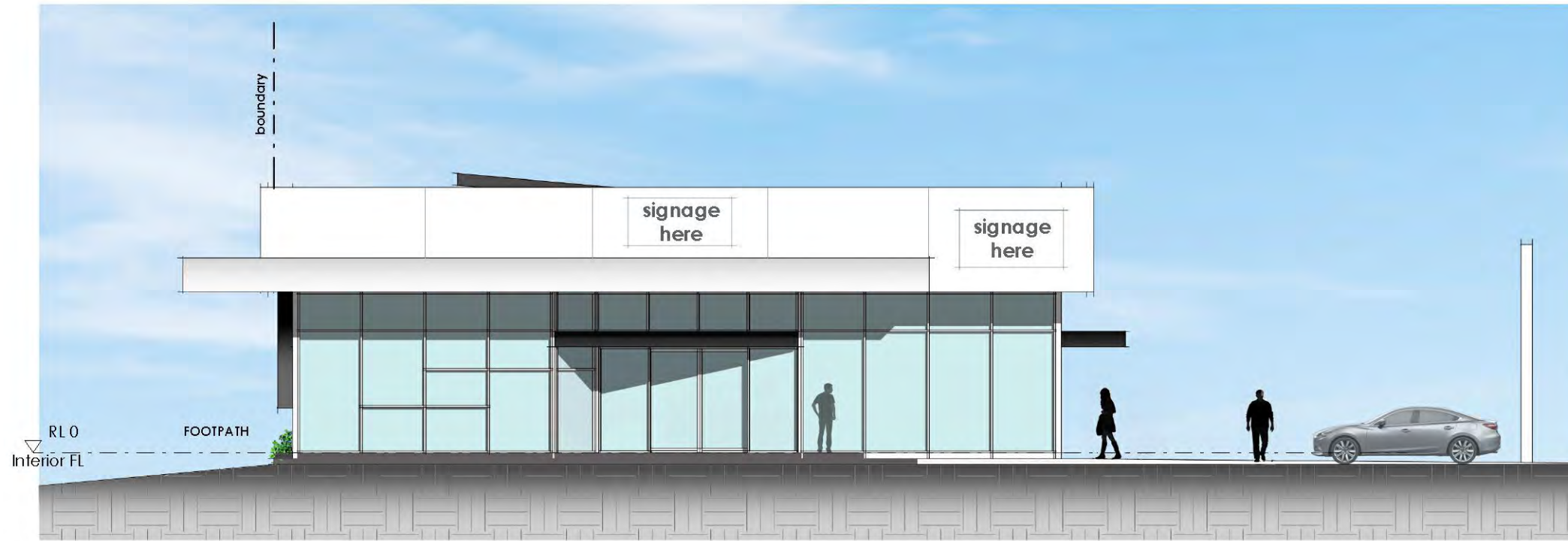
Project:  
New Car Yard Development  
26 Lake Steet, Cambridge.

Client:  
Ingham Driven

Drawing Title:  
Proposed Floor Plan - Building C  
CAD File: Autodesk Docs://561 New Car Yard Development/561 Ingham Cambridge New Development\_Master.rvt

| REV. | DATE     | REVISION DETAILS            |
|------|----------|-----------------------------|
| B    | 27.01.22 | Issued for Resource Consent |
| A    | 12.12.22 | Issued for Discussion       |

|                        |                     |                   |
|------------------------|---------------------|-------------------|
| Scale:<br>1 : 100 @ A3 | Date:<br>12.12.2022 | Drawn:<br>HS      |
| Job No:<br>I561        | Designed:<br>DLA    | Checked:<br>MS    |
| Drawing No:<br>SK2.03  | Revision No:<br>51  | Revision No:<br>B |



1 Building A South Elevation - Queen St  
1 : 150 @ A3



2 Building A West Elevation - Lake Rd  
1 : 150 @ A3

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| REV. | DATE     | REVISION DETAILS            |
|------|----------|-----------------------------|
| D    | 27.01.22 | Issued for Resource Consent |
| C    | 12.12.22 | Issued for Discussion       |
| B    | 6.12.22  | Issued to Client for Review |
| A    | 5.12.22  | DRAFT for Comment           |

|                       |                     |                   |
|-----------------------|---------------------|-------------------|
| Scale:<br>@ A3        | Date:<br>12.12.2022 | Drawn:<br>AJN     |
| Job No:<br>1561       | Designed:<br>DLA    | Checked:<br>MS    |
| Drawing No:<br>SK4.00 | Revision No:<br>52  | Revision No:<br>D |

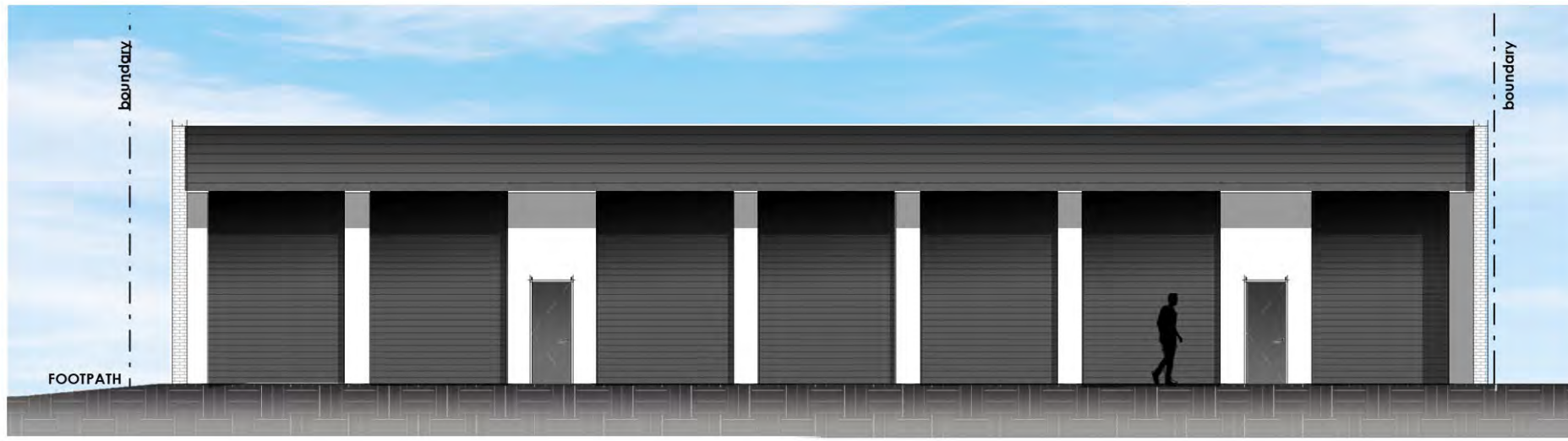


1 Building B South Elevation - Queen St  
1 : 150 @ A3

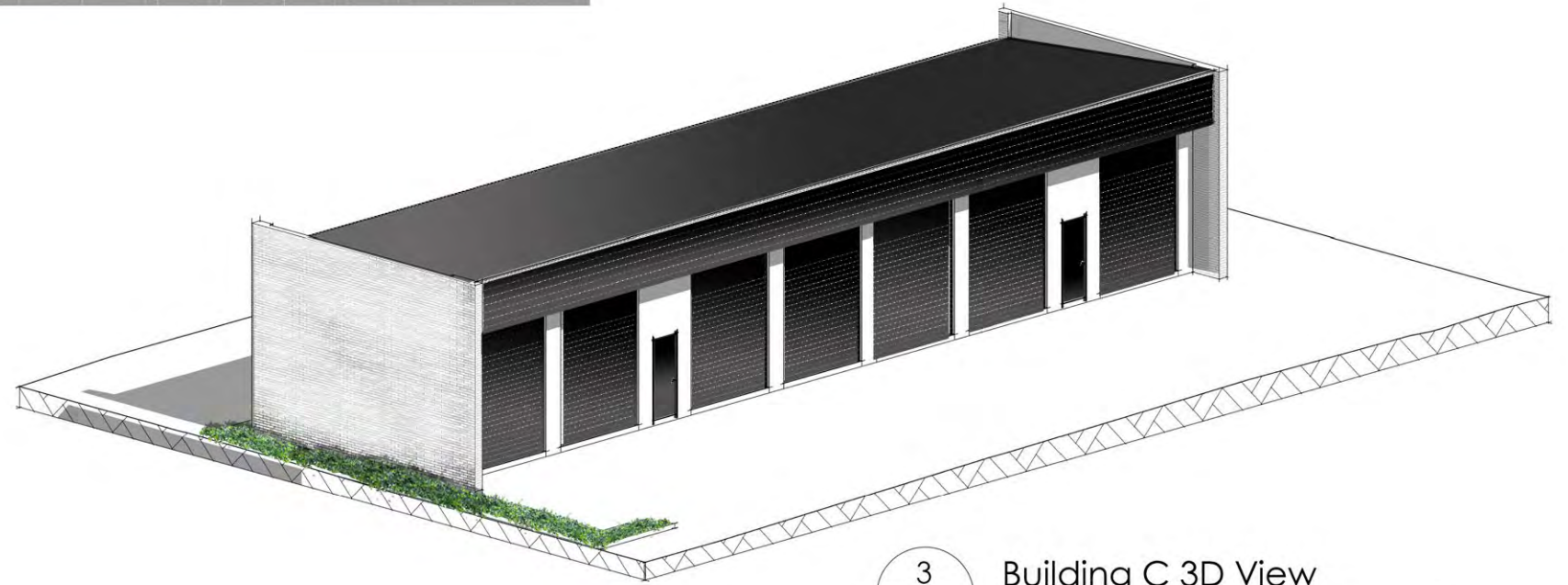


2 Building B West Elevation  
1 : 150 @ A3

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1 Building C South Elevation  
1 : 150 @ A3



3 Building C 3D View  
@ A3



2 Building C North Elevation  
1 : 150 @ A3

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| REV. | DATE     | REVISION DETAILS            |
|------|----------|-----------------------------|
| D    | 27.01.22 | Issued for Resource Consent |
| C    | 12.12.22 | Issued for Discussion       |
| B    | 6.12.22  | Issued to Client for Review |
| A    | 5.12.22  | DRAFT for Comment           |

|                       |                     |                   |
|-----------------------|---------------------|-------------------|
| Scale:<br>@ A3        | Date:<br>12.12.2022 | Drawn:<br>AJN     |
| Job No:<br>1561       | Designed:<br>DLA    | Checked:<br>MS    |
| Drawing No:<br>SK4.05 | Revision No:<br>54  | Revision No:<br>D |





1 Proposed Queen Street Elevation  
1 : 300 @ A3

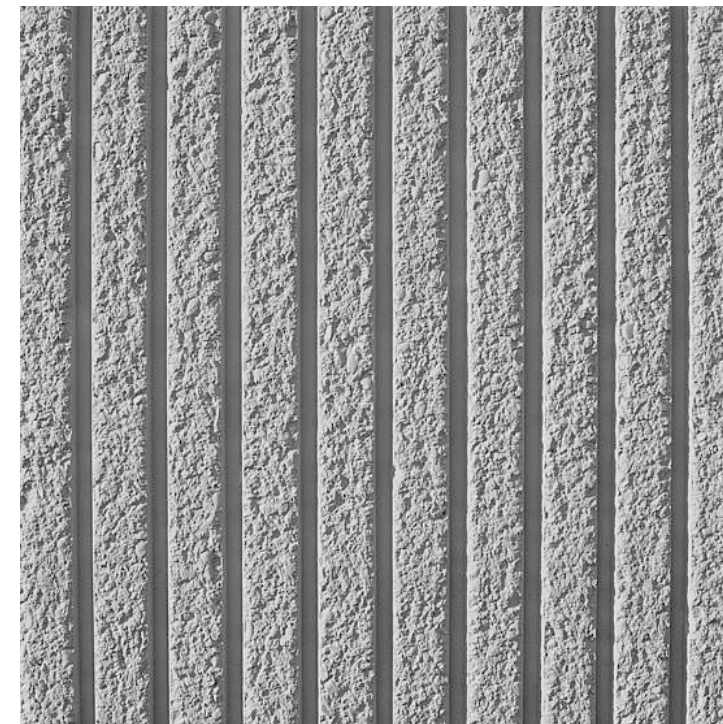


2 Proposed Lake Road Elevation  
1 : 300 @ A3

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resource consent LU/0023/23



precast cladding (textured), glazed sliding entry doors and aluminium composite cladding + canopy



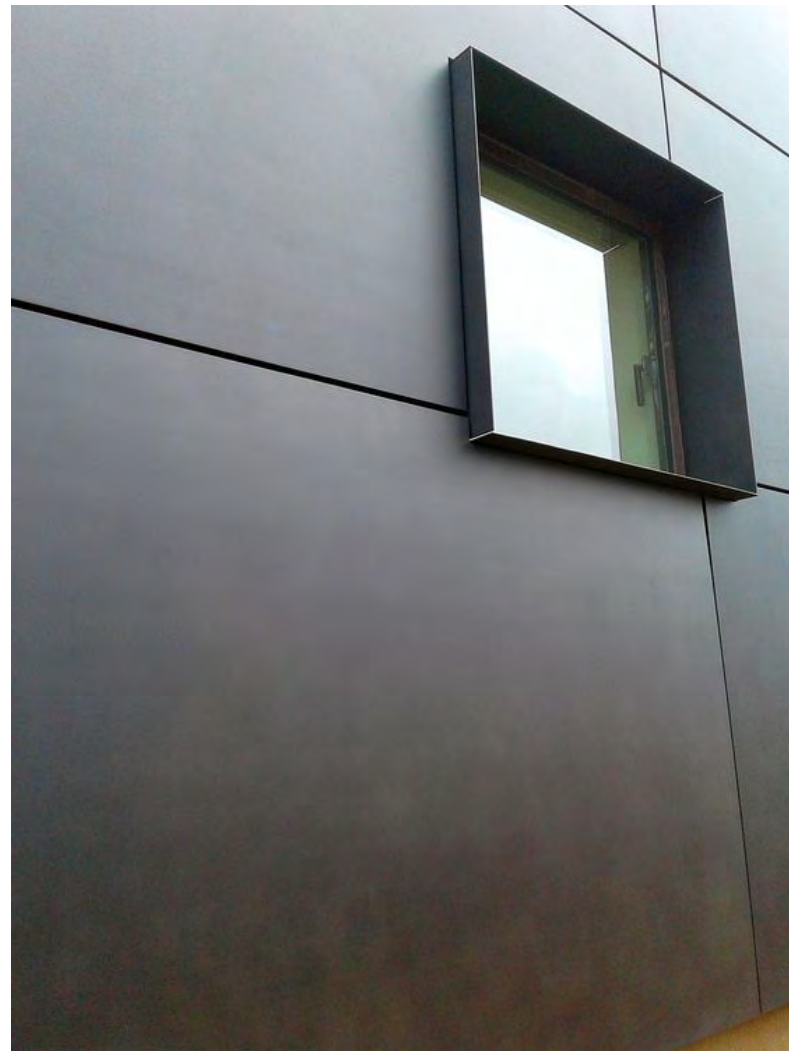
textured precast panels



exposed aggregate exterior slab + steps



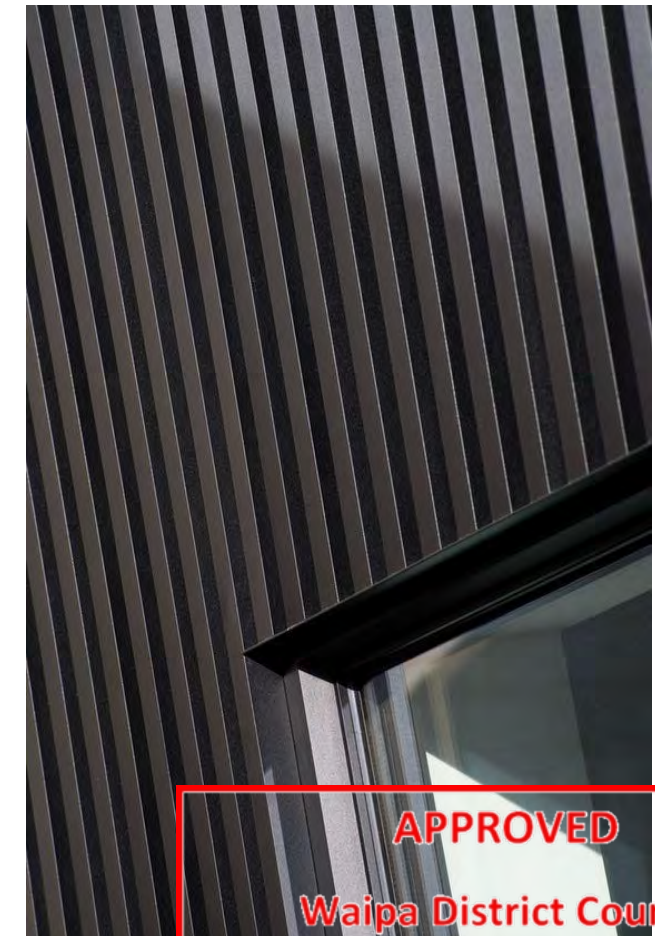
bagged brick cladding paint finish - (building C only)



aluminium composite cladding with feature exterior window shroud



glazed facade, exposed aggregate exterior slab + steps, entry portals recessed back into building footprint



reverse run aluminium cladding

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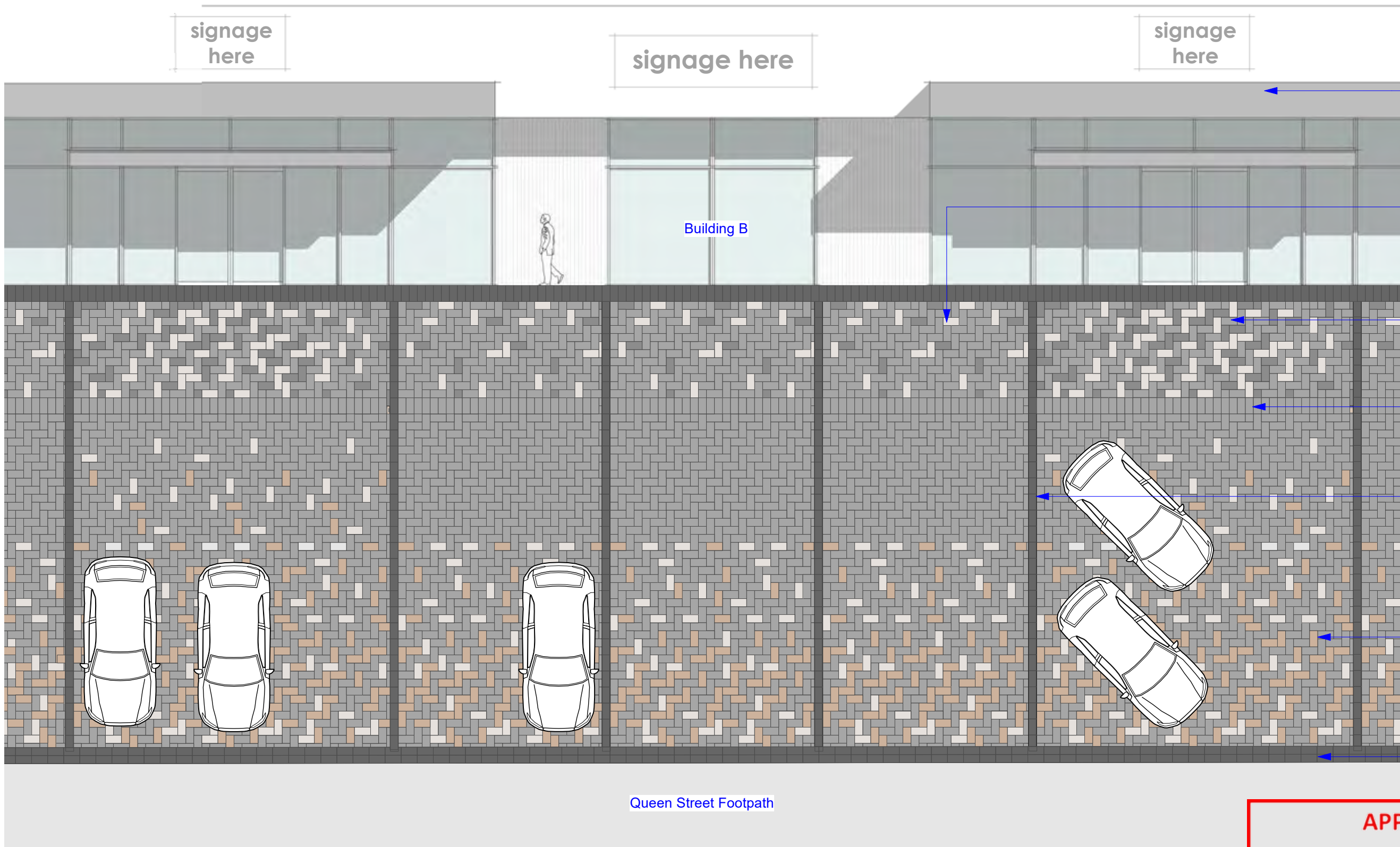
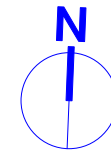


1 Overview Plan  
Scale: 1:400

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The purpose of this plan is to show the general intent of the design and may not be complete in every detail. This plan is not intended as a construction drawing and should not be used as such.





Elevation projection of Building B highlighting the intent of aligning paving with building facade details

Firth Piazza pavers (400 x 200mm). Colour Volcanic Ash, smooth finish, with Glacier and Black Sands accents, honed finish, in herringbone pattern.

Frequency of Glacier and Black Sands colour increases at building entrances

Soldier course of Firth Piazza pavers (400 x 200mm). Colour Volcanic Ash, smooth finish.

Soldier course of Firth Piazza pavers (400 x 200mm) to align with building fenestration details. Colour Black Sands. Smooth Finish.

Firth Piazza pavers (400 x 200mm). Colour Volcanic Ash, smooth finish, with Glacier and Sumner accents, honed finish, in herringbone pattern.

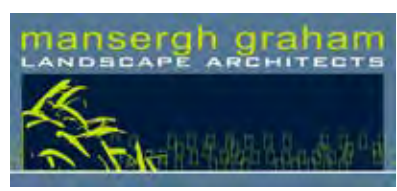
Black stained timber or aluminium bollards. Refer to C06 & C07.

Queen Street Footpath

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**1** Building B Hardscape Detail Plan  
Scale: 1:100

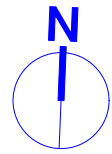
The purpose of this plan is to show the general intent of the design and may not be complete in every detail. This plan is not intended as a construction drawing and should not be used as such.



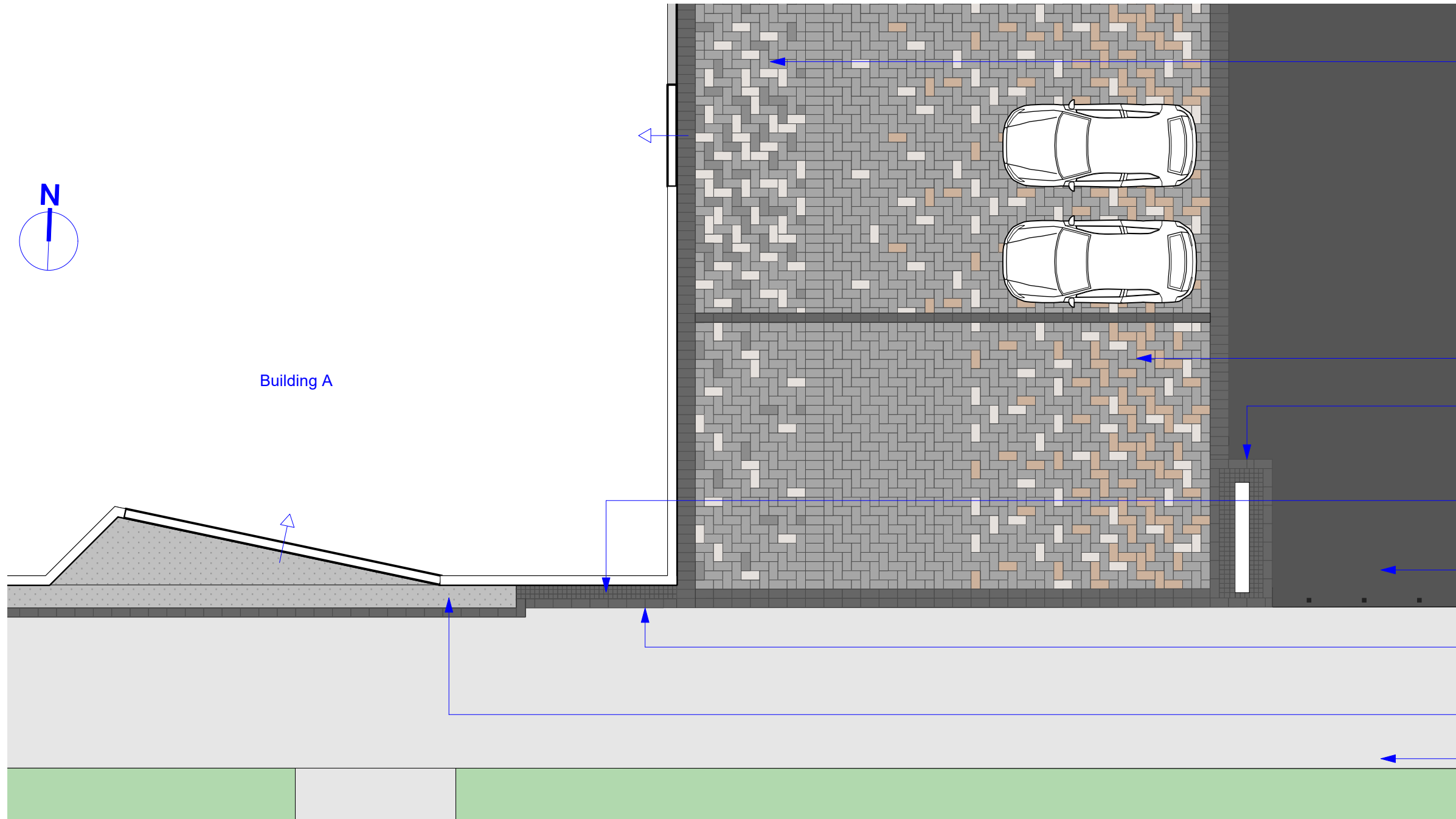
Inghams Cambridge Hardscape Detail

Project No. 2022-042 | Client John Ingham | Phase For Discussion | Scale shown @ A3 | Revision No. R1 | Date 20/01/2023 | Drawn SW | Approved MG

Plan No.  
**C03**  
60



Building A



Firth Piazza pavers (400 x 200mm). Colour Volcanic Ash, smooth finish, with Glacier and Black Sands accents, honed finish, in herringbone pattern. Frequency increasing at entry point.

Firth Piazza pavers (400 x 200mm). Colour Volcanic Ash, smooth finish, with Glacier and Sumner accents, honed finish, in herringbone pattern.

300mm strip of Holland Set pavers, colour Black Sands with single row Piazza pavers around base of pylon signage.

300mm strip of Holland Set pavers, colour Black Sands. Smooth finish.

Asphalt

Single row soldier course of Firth Piazza pavers (400 x 200mm). Colour Black Sands. Smooth finish.

Building step, 500mm wide. Exposed aggregate.

Concrete public footpath

1

### Building A Hardscape Detail Plan

Scale: 1:100

**APPROVED**

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Inghams Cambridge

Project No. 2022-042 | Client John Ingham

| Phase For Discussion

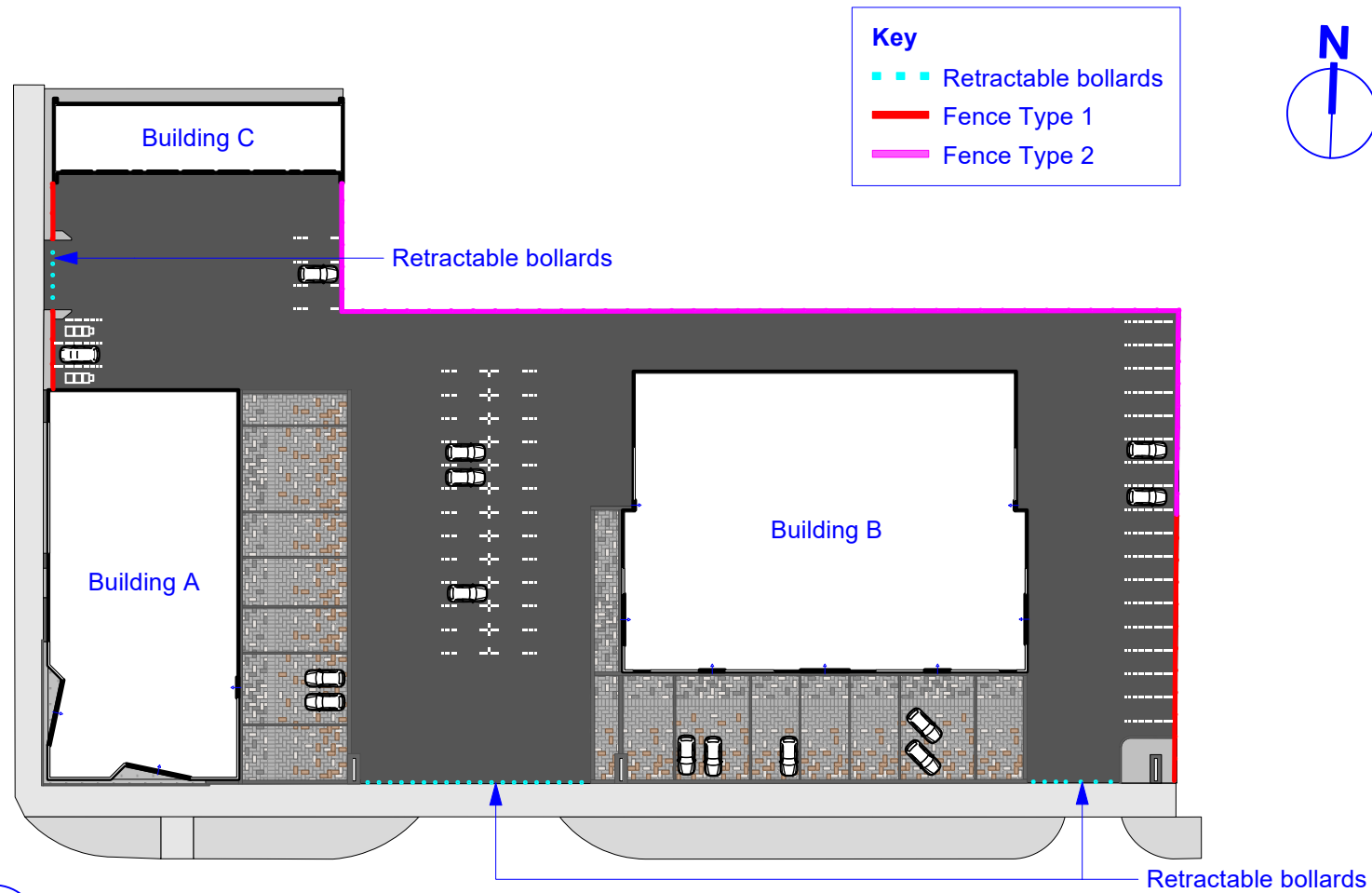
| Scale shown @ A3 | Revision No. R1 | Date 20/01/2023 | Drawn SW | Approved MG

Hardscape Detail

Plan No.

**C04**

61



1 **Fencing Plan**  
Scale: 1:750



2 **Queen Street Front Elevation**  
Scale: 1:200

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Inghams Cambridge

Project No. 2022-042 | Client John Ingham

| Phase For Discussion

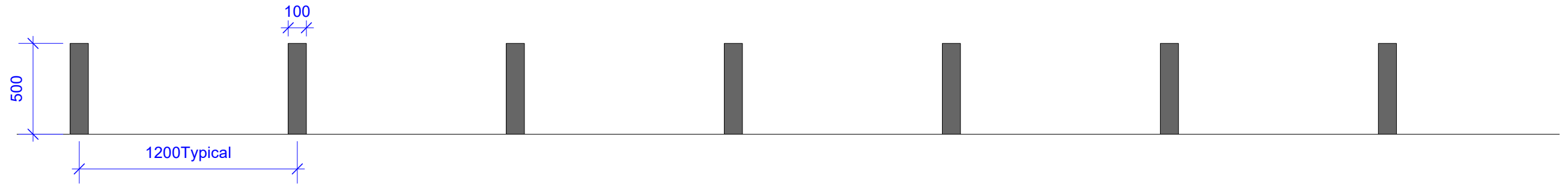
| Scale shown @ A3 | Revision No. R1 | Date 20/01/2023 | Drawn SW | Approved MG

Fencing Plan

Plan No.

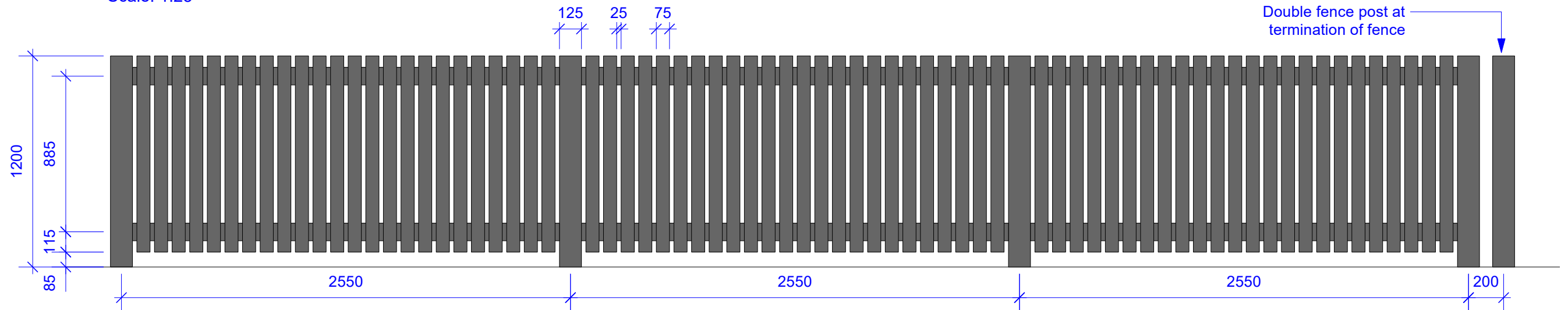
**C05**

62



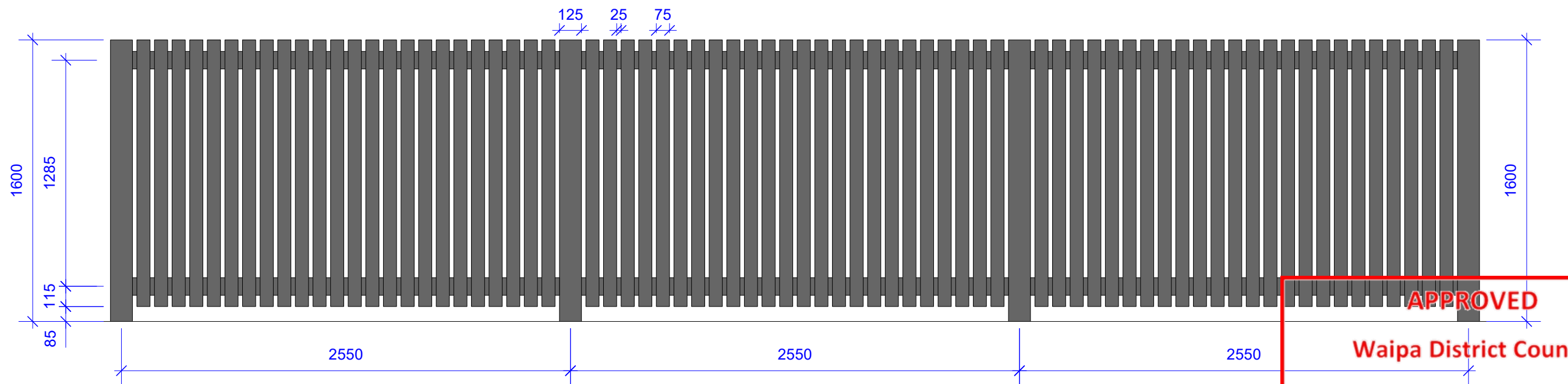
**Retractable bollards**

Scale: 1:25



**Fence Type 1**

Scale: 1:25



**Fence Type 2**

Scale: 1:25

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Inghams Cambridge

Project No. 2022-042 | Client John Ingham

| Phase For Discussion

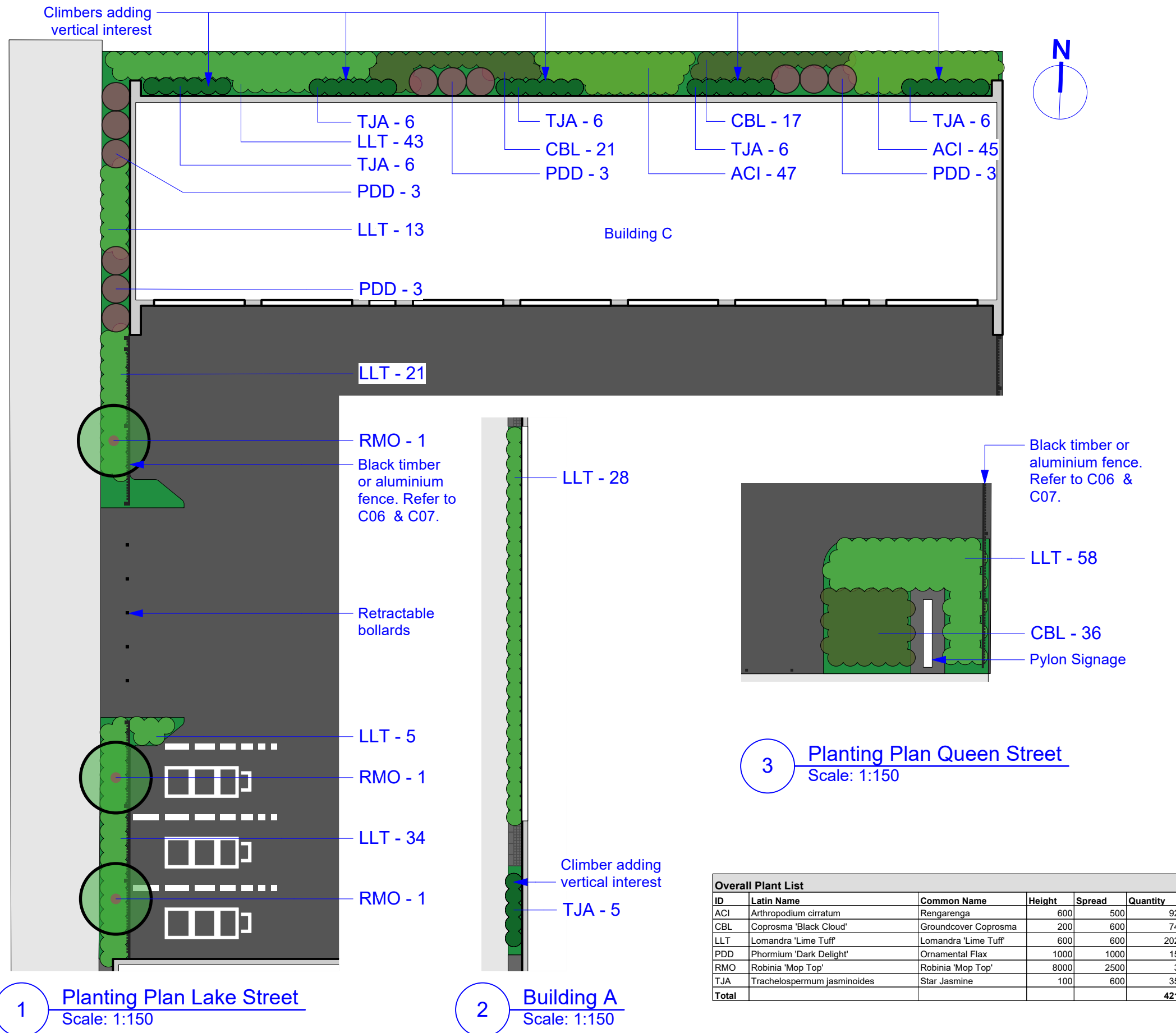
| Scale shown @ A3 | Revision No. R1 | Date 20/01/2023 | Drawn SW | Approved MG

Fencing Details

Plan No.

**C06**

63



**Design intent**

Planting has been used along the western edge of the property to enhance the pedestrian experience down Lake Street as well as the northern edge towards the cinema.

The planting at the Lake St entrance is symmetrical, reflecting the more formal aesthetic of the Cambridge character, with a mix of ground covers shrubs and grasses providing textural and colour variety. A climber is proposed on the building C corner, to break up this less detailed building.

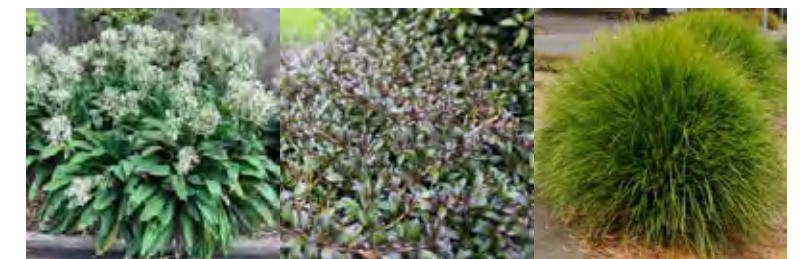
*Robinia 'Mop Top'* trees provide seasonal interest. The fence line on this edge is set back 500mm to align with Building A, allowing planting in front to soften the footpath edge.



Example of climbers giving visual interest to a back wall



Example of understory planting



Arthropodium cirratum Coprosma 'Black Cloud' Lomandra 'Lime Tuff'



Phormium 'Dark Delight' Robinia 'Mop Top' Trachelospermum jasminoides

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| Overall Plant List |                             |                      |        |        |            |
|--------------------|-----------------------------|----------------------|--------|--------|------------|
| ID                 | Latin Name                  | Common Name          | Height | Spread | Quantity   |
| ACI                | Arthropodium cirratum       | Rengarenga           | 600    | 500    | 92         |
| CBL                | Coprosma 'Black Cloud'      | Groundcover Coprosma | 200    | 600    | 74         |
| LLT                | Lomandra 'Lime Tuff'        | Lomandra 'Lime Tuff' | 600    | 600    | 202        |
| PDD                | Phormium 'Dark Delight'     | Ornamental Flax      | 1000   | 1000   | 15         |
| RMO                | Robinia 'Mop Top'           | Robinia 'Mop Top'    | 8000   | 2500   | 3          |
| TJA                | Trachelospermum jasminoides | Star Jasmine         | 100    | 600    | 35         |
| <b>Total</b>       |                             |                      |        |        | <b>421</b> |

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