Residences at 153 Taylor Street

Christopher Beer Architect Limited 32 Victoria Street (rear), Cambridge +64 (0)7 827 9944

studio@christopherbeerarchitect.com

Document Set ID: 11134053

Version: 1, Version Date: 09/11/2023

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Preliminary Design Drawing Set - For RESOURCE CONSENT

Residences at 153 Taylor Street Assured Construction Limited 153 Taylor Street, Cambridge

### **Architectural Design Statement**

# Introduction

This project proposes to develop a residential property located on the Taylor Street greenbelt. The property looks onto McKinnon Park and is within walking and cycling distance to the Cambridge town centre and local schools, shops and public facilities. The architectural design of the project seeks to create high-quality medium-density housing, using building forms and materials appropriate to its location and to create a good example for future residential development within the Cambridge compact housing areas.

### Context

The local area comprises single-storey dwellings on properties of approximately 500-1,000m2, and this property and adjacent properties are designated for compact housing in the District Plan. The area has little recent development and most buildings were constructed from around the 1960s onwards.

We envisage that over the next couple of decades this compact housing stretch of Taylor Street will be developed with units and townhouses, likely in two-storey duplex and terrace forms, and these will provide housing for those seeking compact, low-maintenance homes. This development has been designed to anticipate this future development and to fit with a compact housing and likely medium-density development type.

The property faces north onto Taylor Street, with a width of around 16m, and is bounded on the east and west by right-of-way driveways, which access properties to the rear. The site stretches north-south, providing great solar access into the proposed buildings from the east (morning) and west (afternoon).

### Layout

A group of two-bedroom units with single garages has been designed for the site, and these are grouped in a terrace of five (with a single unit facing the street) and a duplex of two at the rear. The front and rear units have their living areas and outdoor decks/terraces on the ground floor, and the middle four units have upstairs living areas with balconies (facing west), and small additional outdoor areas opening from the ground floor bedrooms

The units are all two-bedroom and two-bathroom, with gross floor areas of between approximately 70–85m² (plus garage). The dwellings all have good daylight access and passive solar gain potential. Units 1-5 have views of McKinnon Park from their outdoor

The buildings have been designed to fit within the height-in-relation-to-boundary recession planes, particularly on the south boundary, to ensure daylight and passive solar gain for the surrounding properties is not diminished. The buildings have been arranged to fit within the internal boundary setbacks as far as possible, and where they infringe on the setbacks, these infringements are confined to right-of-way boundaries, which ensures the buildings don't appear or feel too close to neighbouring buildings.

### Local Development

We've previously designed a similar development at 109 Taylor Street (three blocks to the west), which made use of a similar site layout. This development provided a terrace of 120m2 three-bedroom townhouses at the front of the site and a duplex of 100m2 twobedroom units at the rear. It was completed in 2022 and won an award in the housing category of the New Zealand Institute of Architects Awards (Waikato/BOP) in 2023.

# **Street Facing Unit**

The front unit looks onto the footpath and reserve opposite from its kitchen, living room and upstairs bedroom. It has a generous amount of glazing on this north facade, and its outdoor living terrace and garden are located in the front yard. A low garden wall and low planting will provide a comfortable and pleasant interface between the dwelling and the

The front door of the unit is located on the driveway side of the dwelling, rather than on the street-facing facade. This location provides better continuity with the entrances of the other units, allows entrance into the rear of the kitchen rather than through the living room and avoids having a front door on the street-facing facade, providing more space for windows and a larger glazed sliding door.

# Architectural Design

The units have been designed to balance an appearance of overall continuity and visual cohesiveness, with an individuality for each dwelling—so they appear to be one development while maintaining the feeling of individual 'homes'.

Overall, the buildings have a consistent mono-pitch roof design, with the units being expressed by articulation of the building facades—the walls and roofs stepping in and out around the buildings. This give a consistent appearance and a proportion and rhythm to the architecture. The stepping form of the facades and the shadows created by the recesses and overhangs provide a dynamic and varied appearance, and a domestic scale to the units.

Balconies are provided on the middle group of units. These are located overlooking the shared driveway, which adds visual interest both the building and the driveway, while also providing an 'activation' and community feel to this area. The driveway is a shared vehicle and pedestrian area, with landscaping and planting providing a 'soft' appearance and the narrow width encouraging the slow and safe movement of vehicles.

Entrances to the homes are each defined by a covered front door area and single garage door. Inside, all the living areas have a connection to the outside and all bedrooms and bathrooms are private and well separated from the living areas.

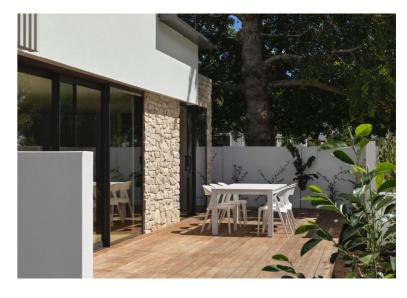
Materials are selected for durability, low-maintenance and to age and weather well. Their various textures and colours will provide a varied and high-quality appearance.

### **Development Precedent Images**

Existing townhouse development at 109 Taylor Street







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32 Victoria Street (rear), Cambridge +64 (0)7 827 9944 **Assured Construction Limited** studio@christopherbeerarchitect.com Residences at 153 Taylor Street 153 Taylor Street, Cambridge

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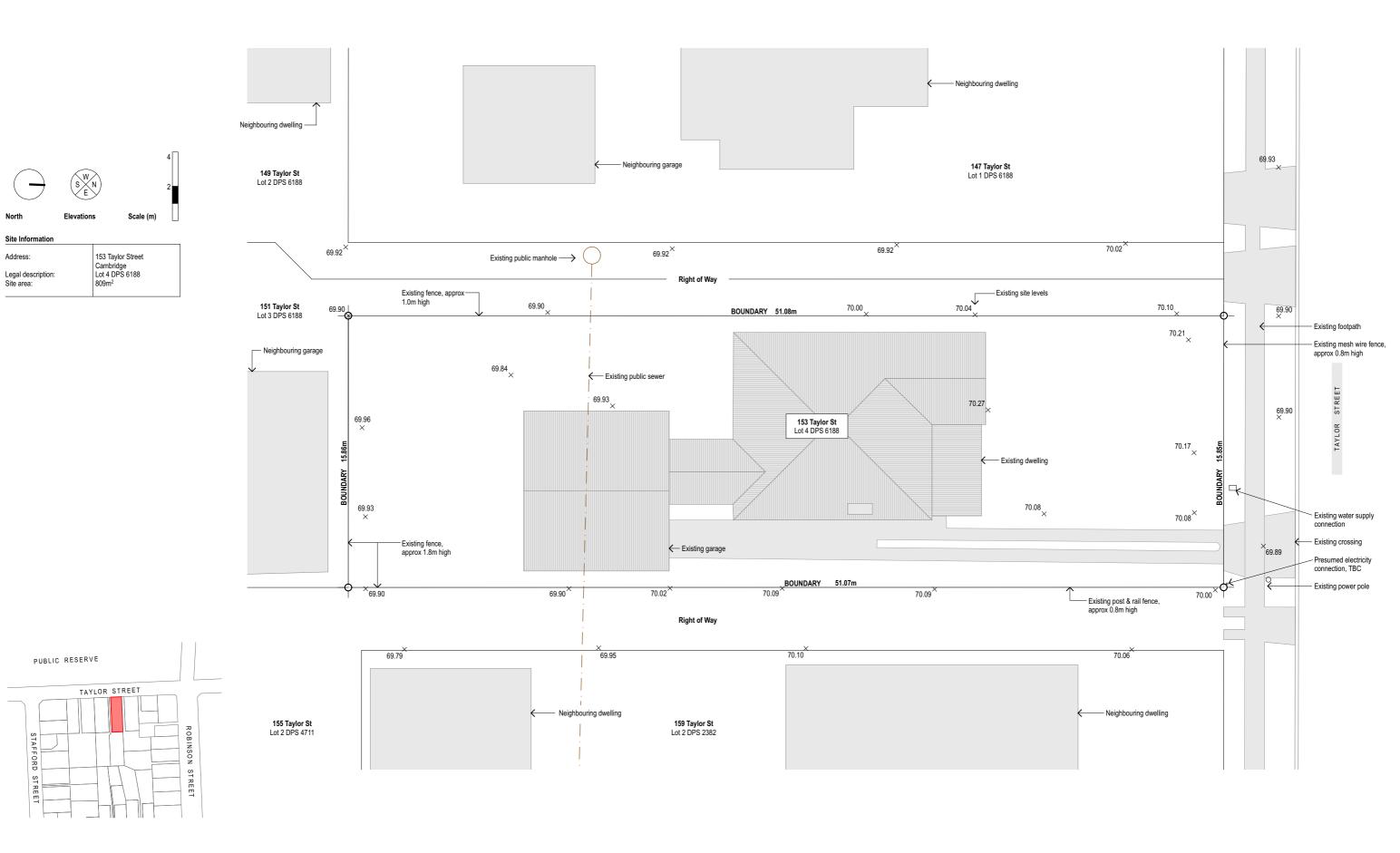
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Architectrual Design Statement

**SK.00(A)** Sheet number (revision):

Drawing issued: 24/10/23 Drawing scale:

**Christopher Beer Architect Limited** 



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Document Set ID: 11134053 Version: 1, Version Date: 09/11/2023

Address:

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**Assured Construction Limited** Residences at 153 Taylor Street 153 Taylor Street, Cambridge

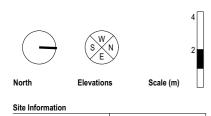
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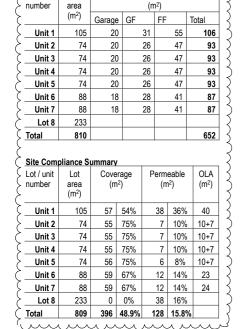
**PLAN: Existing Site** 

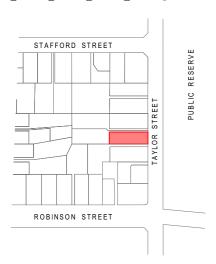
**SK.01(B)** Sheet number (revision):

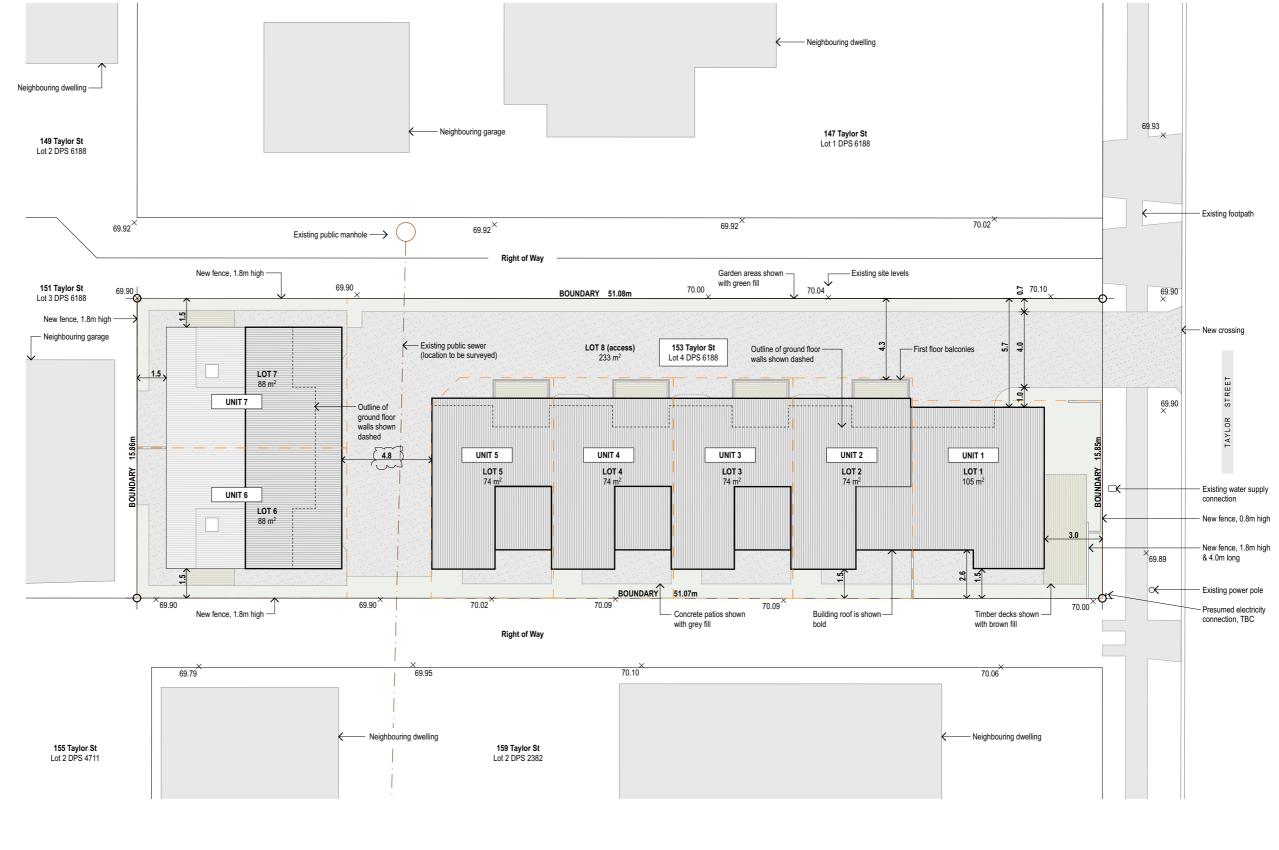


153 Taylor Street Address: Cambridge Lot 4 DPS 6188 Legal description: Site area:

**Buildings/Lots Summary** Lot / unit Lot







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PLAN: Site

SK.02(G) Sheet number (revision):



Address:	153 Taylor Street Cambridge
Legal description:	Lot 4 DPS 6188
Site area:	809m <sup>2</sup>

√√√ Buildings	/Lots Sur	nmary	$\sim$	$\sim$	$\sim$	$\sim$	$\sim$	$\sim$
Lot / unit number	Lot area	illiai y	Gro		oor :	area		
	(m <sup>2</sup> )	Garage	GF	F FF			Tot	al
Unit 1	105	20	)	31		55		106
Unit 2	74	20	)	26		47		93
Unit 3	74	20	)	26		47		93
Unit 4	74	20	)	26		47		93
Unit 5	74	20	)	26		47		93
Unit 6	88	18	3	28		41		87
Unit 7	88	18	3	28		41		87
Lot 8	233							
Total	810							652
Site Com Lot / unit number								
Unit 1	105	57	54%		38	36%		40
Unit 2	74	55	75%		7	10%	1	10+7
Unit 3	74	55	75%		7	10%	1	10+7
Unit 4	74	55	75%		7	10%	<u> </u>	10+7
Unit 5	74	56	75%		6	8%	1	10+7
Unit 6	88	59	67%		12	14%	_	23
Unit 7	88	59	67%		12	14%	_	24
Lot 8	233	0	0%		38	16%		

809 396 48.9% 128 15.8%

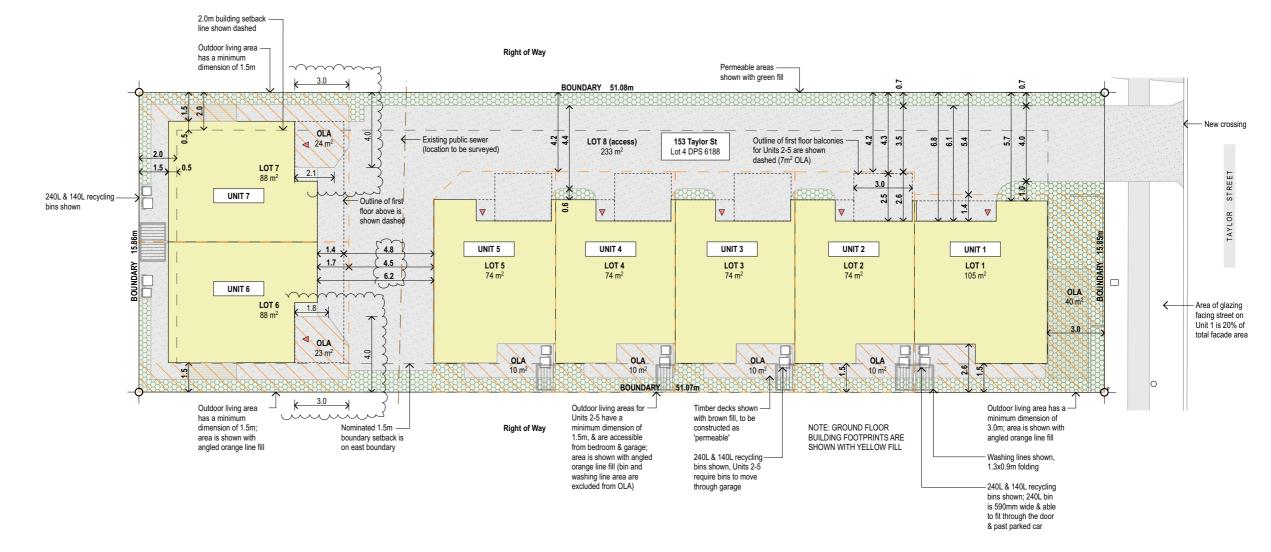
Total



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**Assured Construction Limited** Residences at 153 Taylor Street 153 Taylor Street, Cambridge

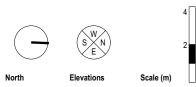
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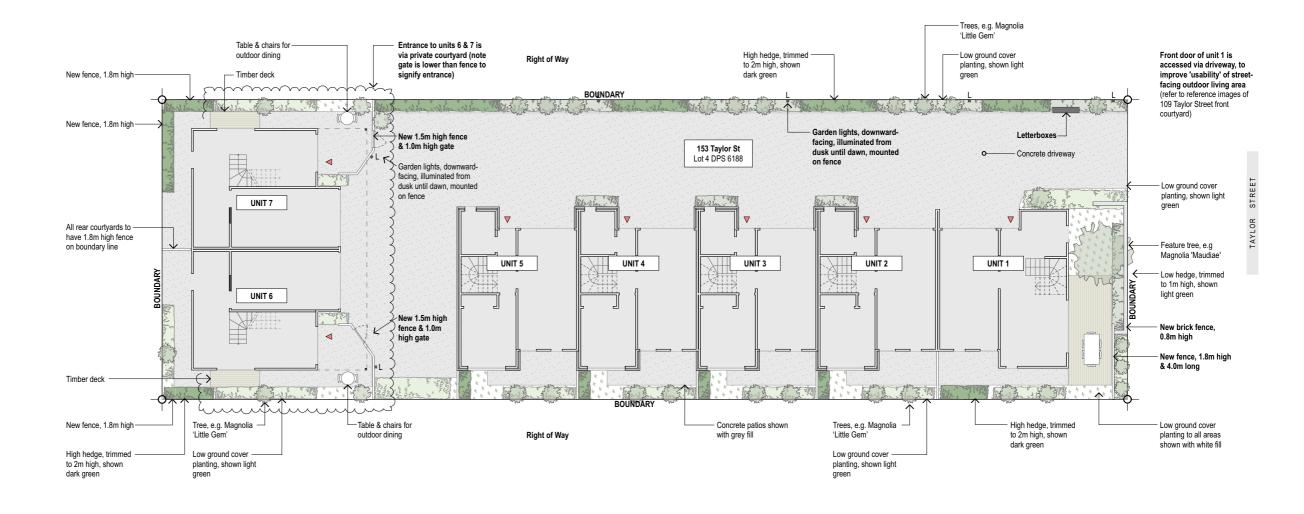
PLAN: Site Development Restrictions & Compliance

SK.03(L)

Sheet number (revision):



153 Taylor Street Address: Cambridge Lot 4 DPS 6188 Legal description: Site area:



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**Assured Construction Limited** 

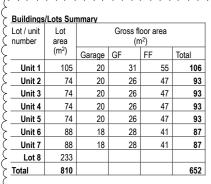
Residences at 153 Taylor Street

153 Taylor Street, Cambridge

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Architect Limited.

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о Ф 4 - - - • 44041 ч д Ф С С доди Ф ч и ч с т PLAN: Unit 1-5 GF Floor Plans



Buildings	Lots Sur	nmary					
Lot / unit number	Lot area		Gross floor area (m²)				
	(m <sup>2</sup> )	Garage	GF	FF	Total		
Unit 1	105	20	31	55	106		
Unit 2	74	20	26	47	93		
Unit 3	74	20	26	47	93		
Unit 4	74	20	26	47	93		
Unit 5	74	20	26	47	93		
Unit 6	88	18	28	41	87		
Unit 7	88	18	28	41	87		
Lot 8	233						
Total	810				652		

2.0m building setback -line shown dashed BOUNDARY 51.08m BALCONY BALCONY BALCONY BALCONY 2.9x2.4m 2.9x2.4m KITCHEN 2.9x2.4m KITCHEN 2.9x2.4m KITCHEN 2.9x2.4m KITCHEN 2.9x2.4m STUDY 3.5x2.4m 3.1x3.4m LIVING 4.0x4.5m LIVING LIVING LIVING 4.0x4.5m 4.0x4.5m 4.0x4.5m BED 2.9x3.1m BED 2.9x3.1m BED 2.9x3.1m BED 2.9x3.1m BED 3.5x3.0m BOUNDARY 1.5m building setback

UNIT 3

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UNIT 4

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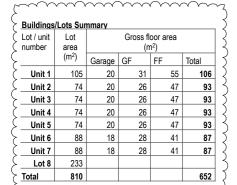
UNIT 2

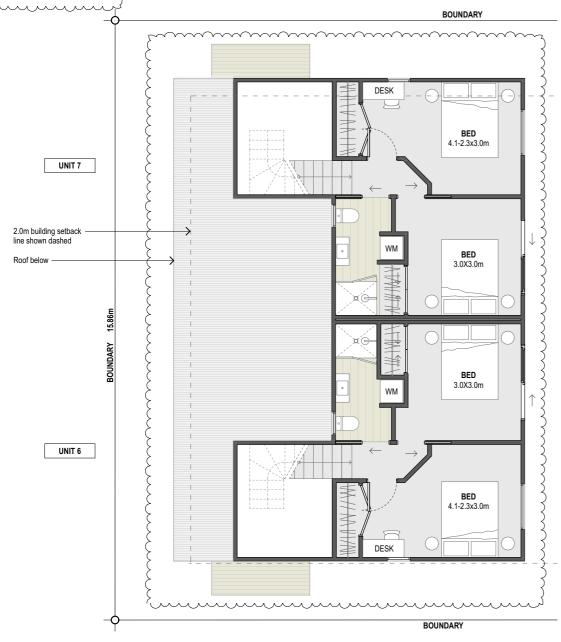
line shown dashed

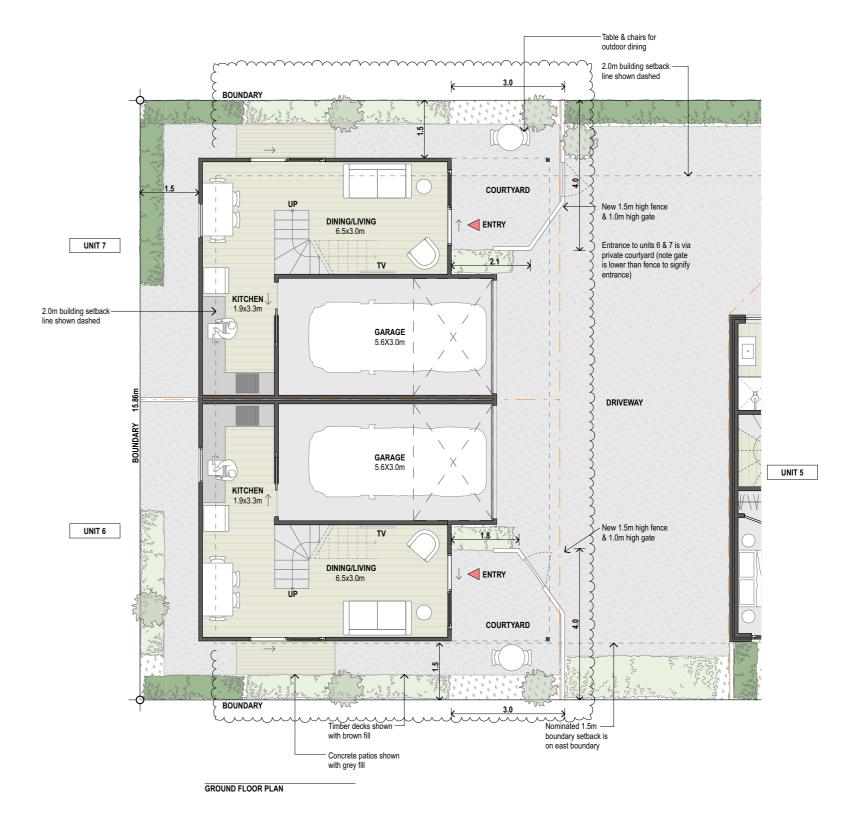
PLAN: Unit 1-5 FF Floor Plans

SK.06(I)
Sheet number (revision):

UNIT 1







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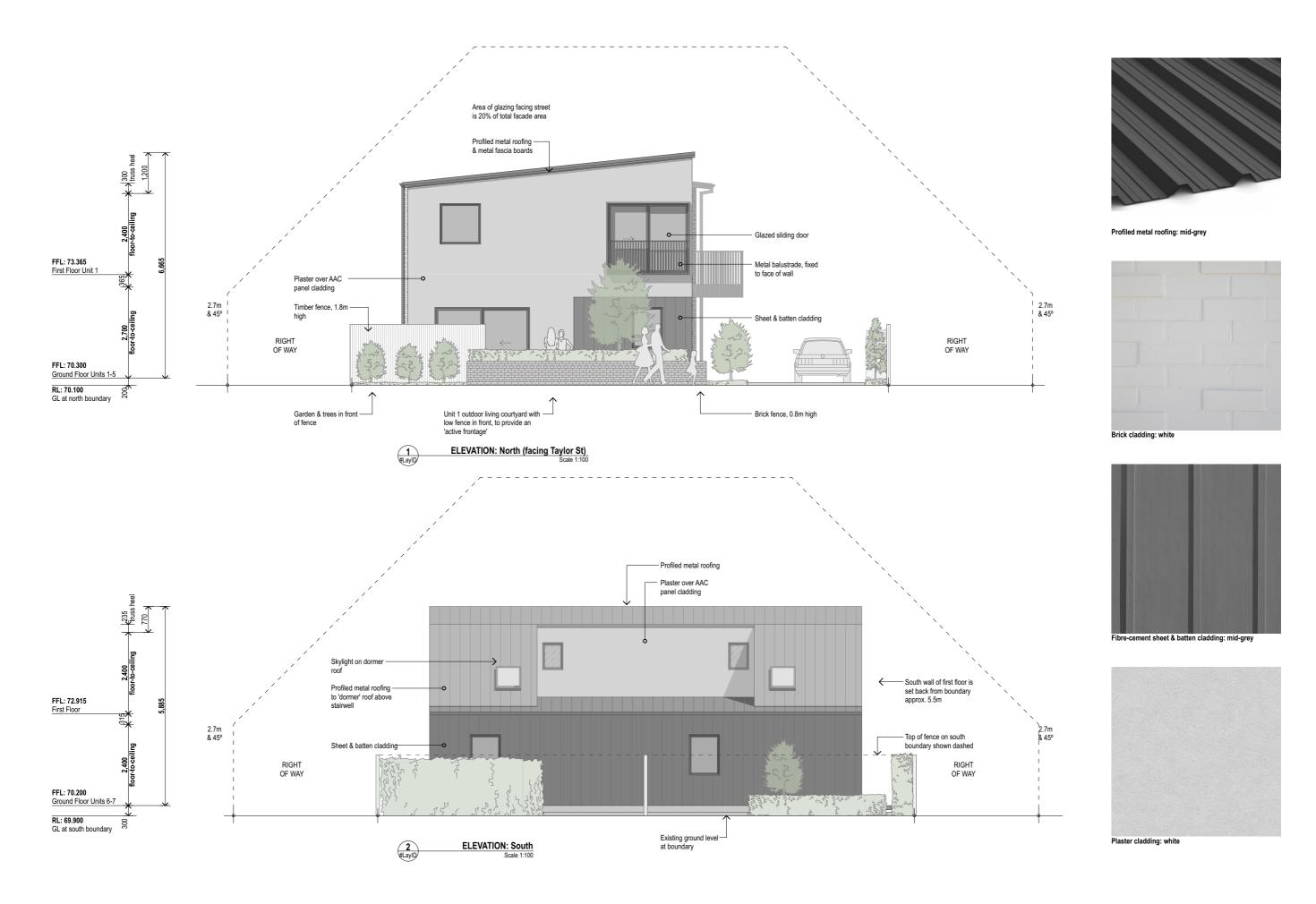
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FIRST FLOOR PLAN

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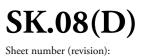
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υ Ф Н - Н • · 4 년 0 년 1 ч д Ф С С доди Ф ч и ч с т **ELEVATIONS: North & South** 





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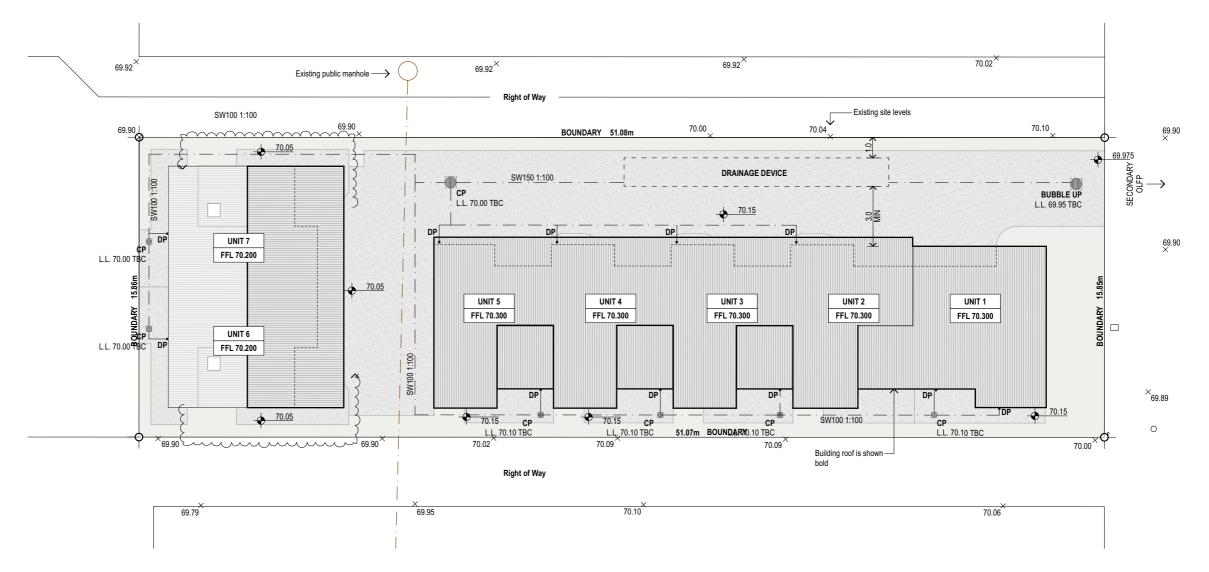
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**ELEVATIONS: East & West** 

**SK.09(C)** Sheet number (revision):





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PLAN: Site Drainage

Sheet number (revision):

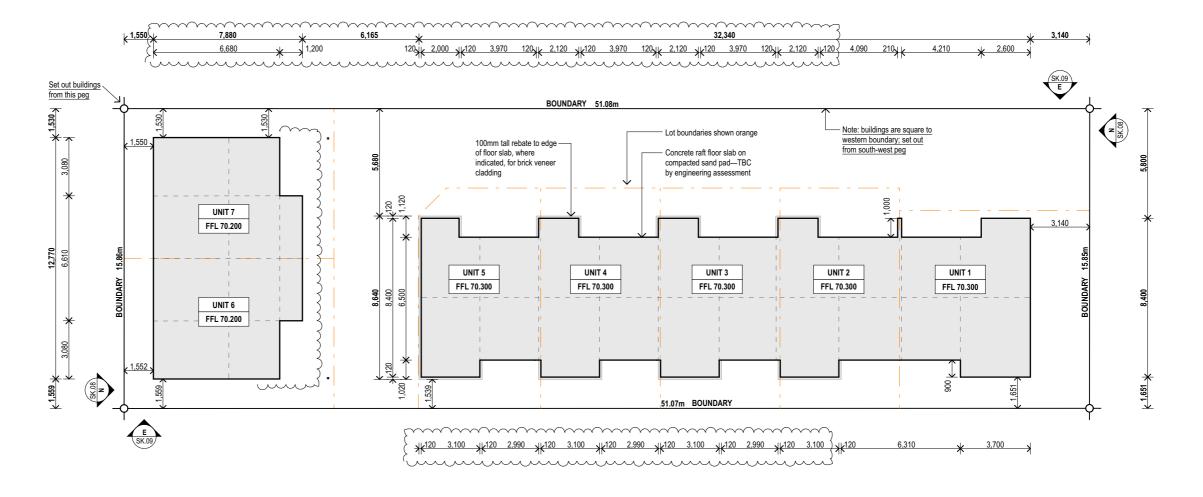


### Site Information

Address: Legal description:

Site area:

153 Taylor Street Cambridge Lot 4 DPS 6188 809m<sup>2</sup>



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PLAN: Ground Floor Slab & Foundation

SK.21(C)
Sheet number (revision):



Address: Legal description: Site area:

153 Taylor Street Cambridge Lot 4 DPS 6188 809m<sup>2</sup>

# Vehicle manoeuvring for Units 1-5:

- Tracking shown for AS/NZS 2890 B99 Car
- Garage door opening complies with AS/NZS 2890 Figure 5.4, with width of doorway at 2.8m & width of driveway (apron width) greater than 6.0m (includes 300mm reduction due to garden on opposite side)



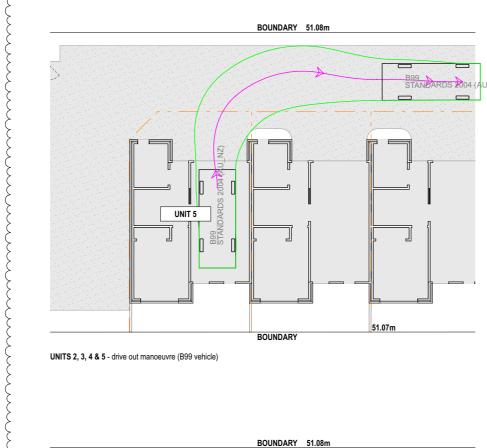
# B99

	Meters
Width	: 1.94
Track	: 1.84
Lock to Lock Time	: 6.0 s
Steering Angle	: 33.9 deg

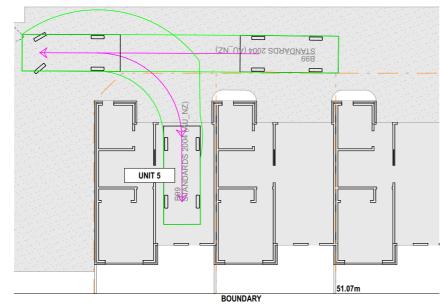
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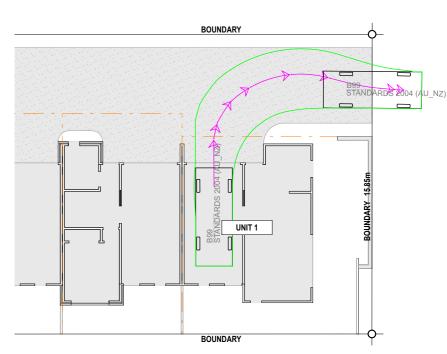
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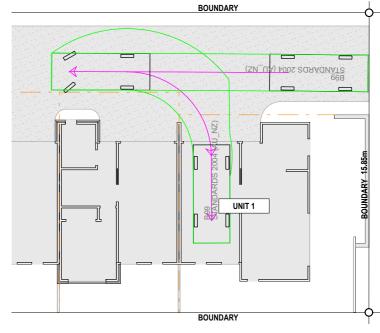




UNITS 2, 3, 4 & 5 - reverse in manoeuvre (B99 vehicle)



UNIT 1 - drive out manoeuvre (B99 vehicle)

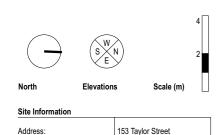


UNIT 1 - reverse in manoeuvre (B99 vehicle)

Preliminary Design Drawing Set - For RESOURCE CONSENT

**Assured Construction Limited** Residences at 153 Taylor Street 153 Taylor Street, Cambridge

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Legal description: Site area: Cambridge Lot 4 DPS 6188 809m<sup>2</sup>

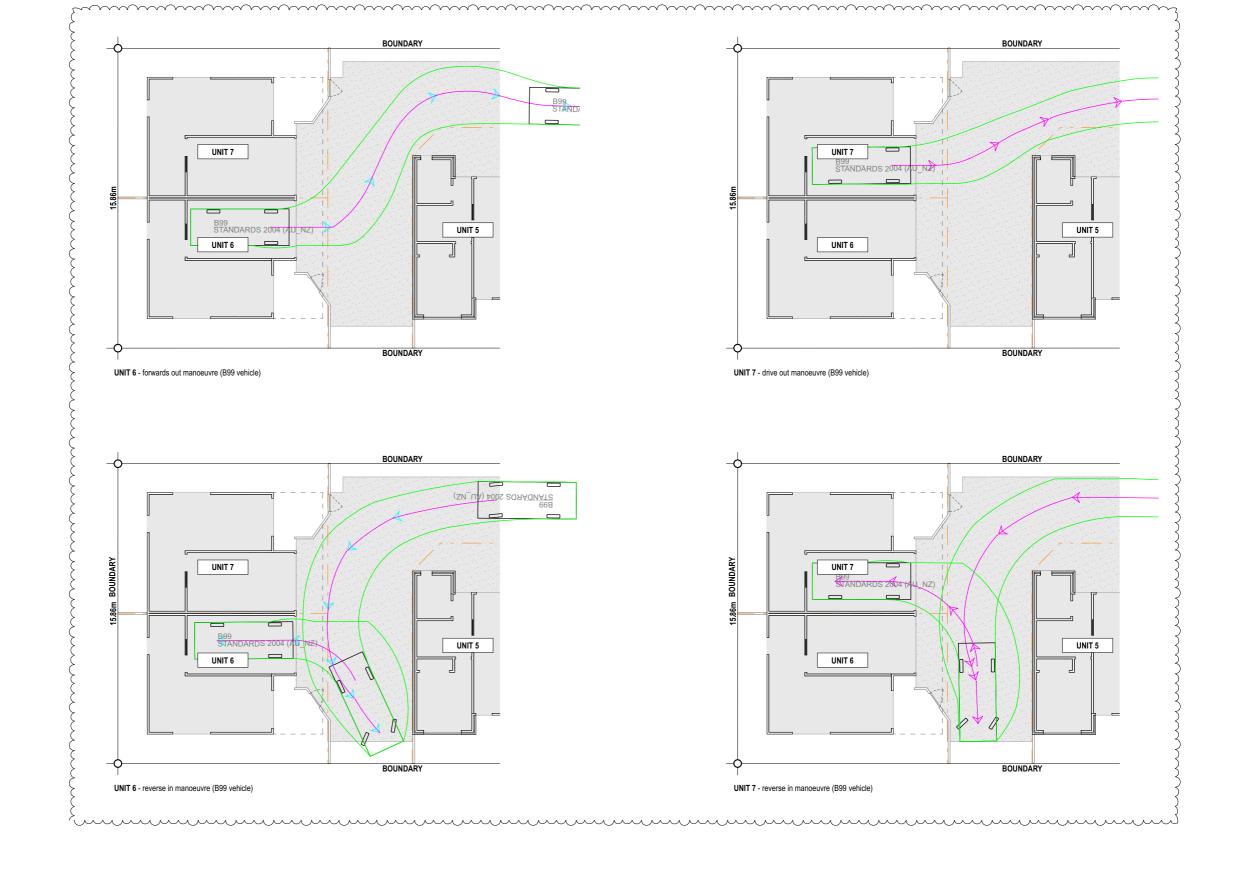
# Vehicle manoeuvring for Unit 6 & 7:

- Tracking shown for AS/NZS 2890 B99 Car
- Garage door opening complies with AS/NZS 2890 Figure 5.4, with width of doorway at 3.0m & width of driveway (apron width) greater than 5.7m



# B99

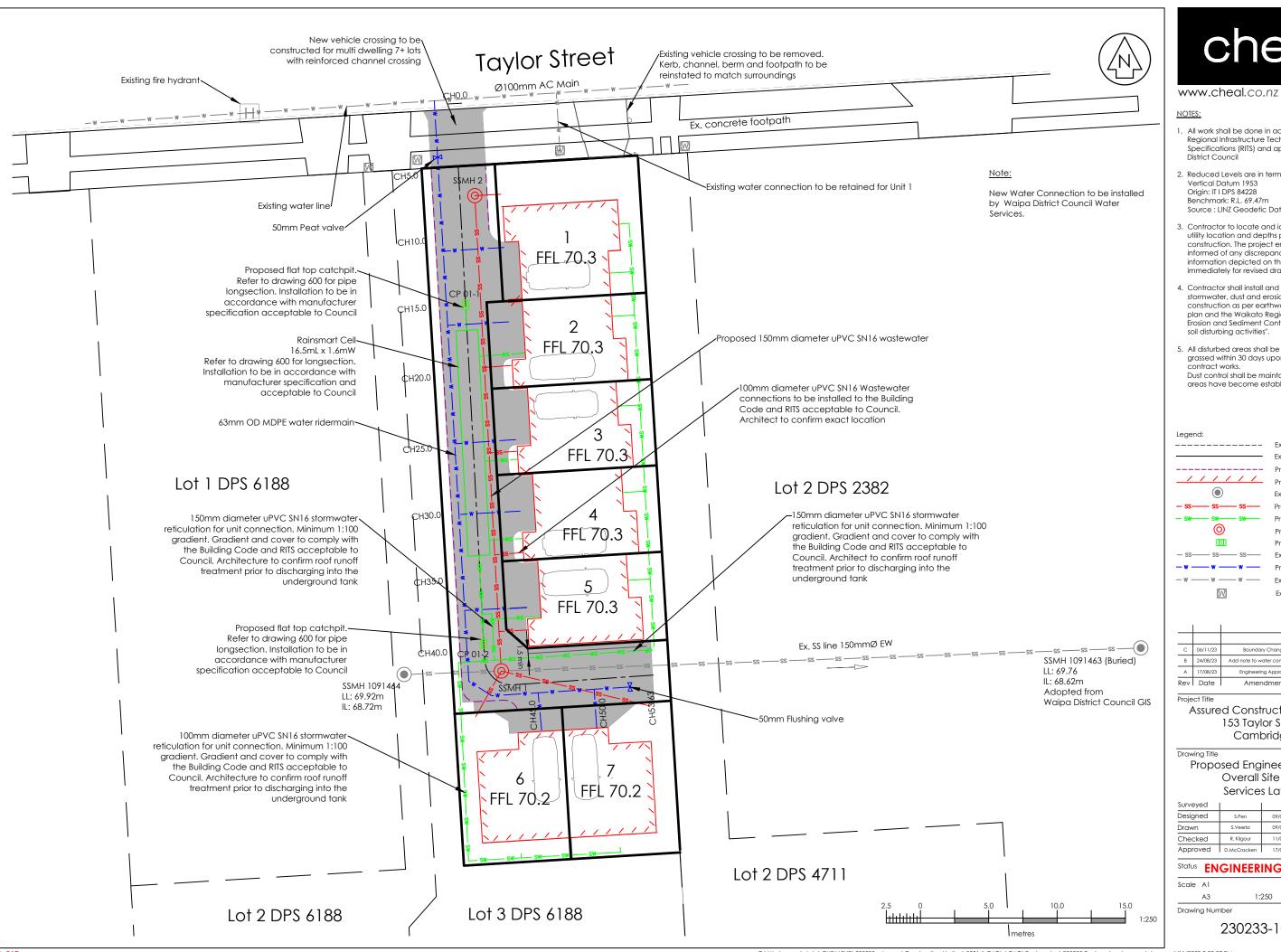
	Meters
Width	: 1.94
Track	: 1.84
Lock to Lock Time	: 6.0 s
Steering Angle	: 33.9 de



Christopher Beer Architect Limited 32 Victoria Street (rear), Cambridge

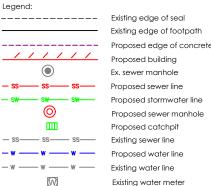
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Document Set ID: 11134053 Version: 1, Version Date: 09/11/2023





- 1. All work shall be done in accordance with the Specifications (RITS) and approval of Waipa
- 2. Reduced Levels are in terms of Moturiki Vertical Datum 1953 Origin: IT I DPS 84228 Source: LINZ Geodetic Database
- 3. Contractor to locate and identify existing utility location and depths prior to construction. The project engineer shall be informed of any discrepancies to the information depicted on these plans immediately for revised drawings.
- 4. Contractor shall install and maintain all stormwater, dust and erosion control during construction as per earthworks management plan and the Waikato Regional Council Erosion and Sediment Control - "Guidelines for soil disturbing activities".
- 5. All disturbed areas shall be re-topsoiled and grassed within 30 days upon completion of
- Dust control shall be maintained until grassed areas have become established.



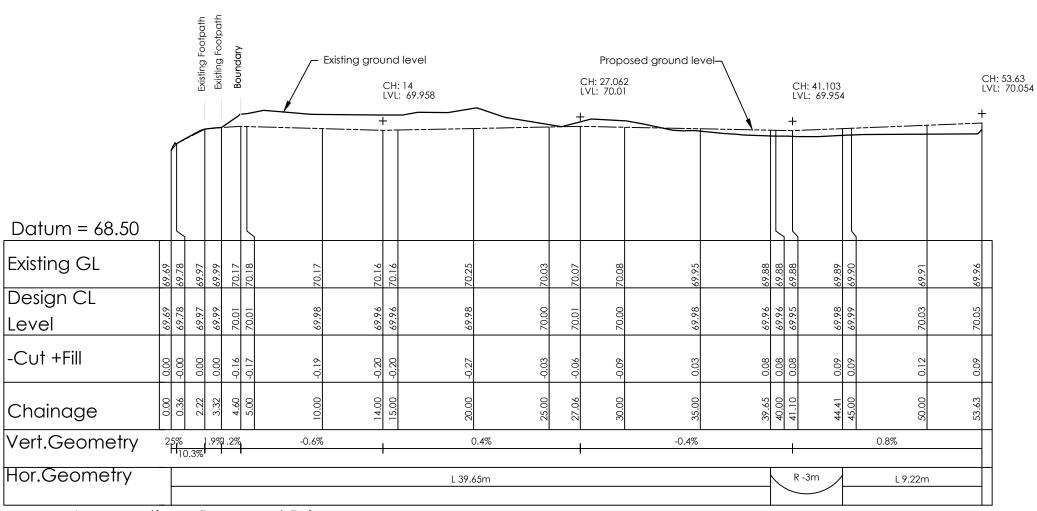
С	06/11/23	Boundary Change	ZM	DVM	DVM
В	24/08/23	Add note to water connection	sv	DVM	DVM
A	17/08/23	Engineering Approval	sv	RFK	DVM
Rev	Date	Amendment	Ву	Chk	App

Assured Construction Limited 153 Taylor Street Cambridge

**Proposed Engineering Works** Overall Site Plan Services Layout

Designed	S.Pen	09/08/23	SP
Drawn	S.Veerla	09/08/23	SV
Checked	R. Kilgour	11/08/23	RFK
Approved	D.McCracken	17/08/23	DVM

Status El	NGINEERING APP	RC	OVAL
Scale A1		ī	A3
A3	1:250	ı	AS
Drawing No	umber	Τ	Rev
	230233-100		С



Longsection - Proposed Driveway SCALES: HOR 1:250 @ A3 VERT 1:50 @ A3



# www.cheal.co.nz

- All work shall be done in accordance with the Regional Infrastructure Technical Specifications (RITS) and approval of Waipa District Council
- Reduced Levels are in terms of Moturiki Vertical Datum 1953 Origin: IT I DPS 84228 Benchmark: R.L. 69.47m Source: LINZ Geodetic Database
- 3. Contractor to locate and identify existing utility location and depths prior to construction. The project engineer shall be informed of any discrepancies to the information depicted on these plans immediately for revised drawings.
- 4. Contractor shall install and maintain all stormwater, dust and erosion control during construction as per earthworks management plan and the Waikato Regional Council Erosion and Sediment Control - "Guidelines for soil disturbing activities".
- 5. All disturbed areas shall be re-topsoiled and grassed within 30 days upon completion of contract works. Dust control shall be maintained until grassed areas have become established.

	Α	11/08/23	Engineering Approval	SV	RFK	DVM
R	ev	Date	Amendment	Ву	Chk	App

# Project Title

Assured Construction Limited 153 Taylor Street Cambridge

Drawing Title

Proposed Engineering Works ROW Long Section

Surveyed		1	1
Designed	S.Pen	09/08/23	SP
Drawn	S.Veerla	09/08/23	sv
Checked	R. Kilgour	11/08/23	RFK
Approved	D.McCracken	11/08/23	DVM

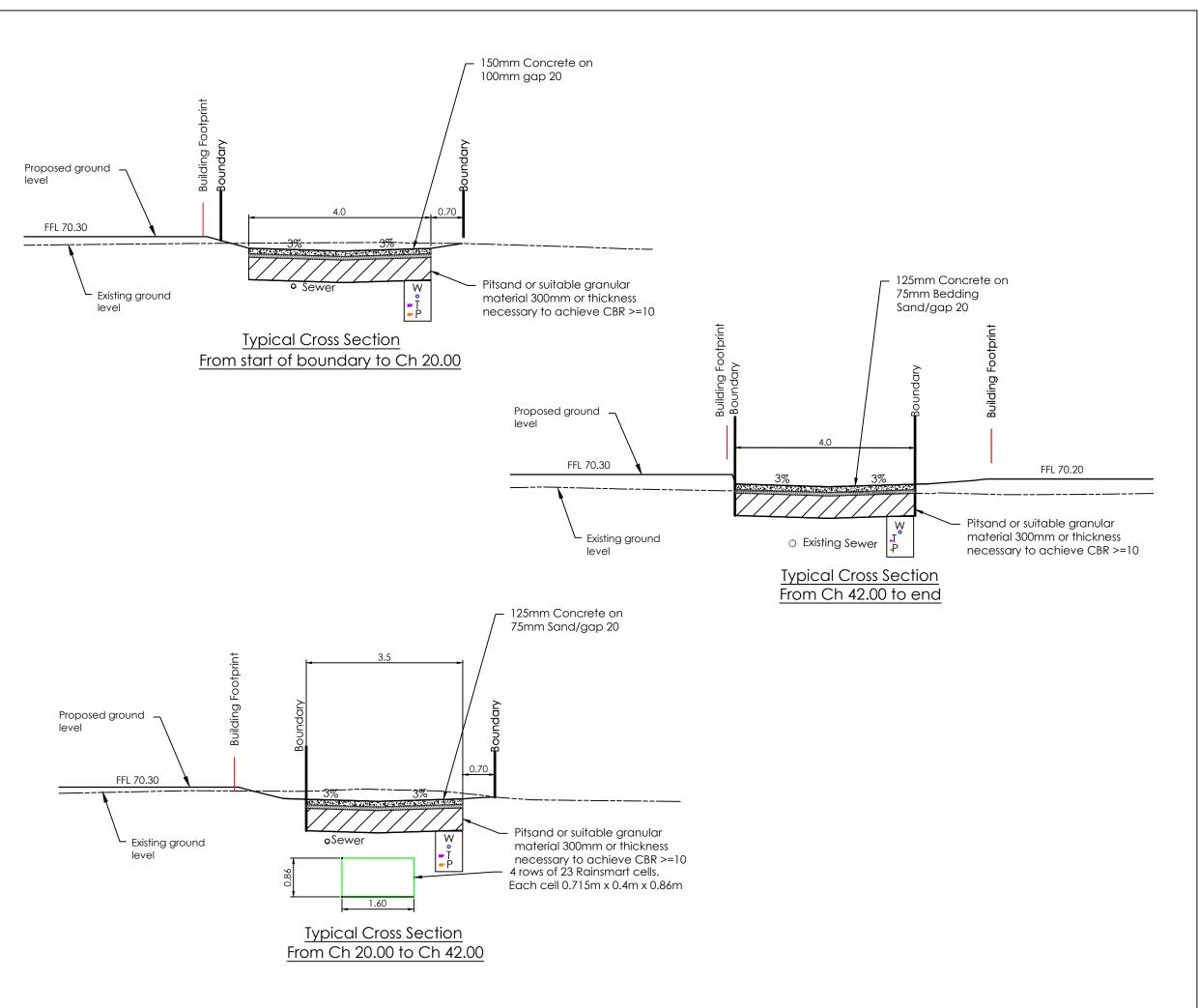
Status ENGINEERING APPROVAL

Scale A1 A3

As Shown Drawing Number

230233-301

| A3





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# NOTES:

- 1. All work shall be done in accordance with the Regional Infrastructure Technical Specifications (RITS) and approval of Waipa District Council
- 2. Reduced Levels are in terms of Moturiki Vertical Datum 1953 Origin: IT I DPS 84228 Benchmark: R.L. 69.47m Source: LINZ Geodetic Database
- 3. Contractor to locate and identify existing utility location and depths prior to construction. The project engineer shall be informed of any discrepancies to the information depicted on these plans immediately for revised drawings.
- 4. Contractor shall install and maintain all stormwater, dust and erosion control during construction as per earthworks manage plan and the Waikato Regional Council Erosion and Sediment Control - "Guidelines for soil disturbing activities".
- 5. All disturbed areas shall be re-topsoiled and grassed within 30 days upon completion of contract works.

  Dust control shall be maintained until grassed

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ľ	Α	11/08/23	Engineering Approval	SV	RFK	DVM
•	Rev	Date	Amendment	Ву	Chk	App

# Project Title

Assured Construction Limited 153 Taylor Street Cambridge

Drawing Title

Proposed Engineering Works **ROW Typical Cross Sections** 

Surveyed			
Designed	S.Pen	09/08/23	SP
Drawn	S.Veerla	09/08/23	SV
Checked	R. Kilgour	11/08/23	RFK
Approved	D.McCracken	11/08/23	DVM

Status ENGINEERING APPROVAL Scale A1

A3 Drawing Number

230233-302



- 1. All work shall be done in accordance with the Regional Infrastructure Technical Specifications (RITS) and approval of Waipa District Council
- Reduced Levels are in terms of Moturiki Vertical Datum 1953 Origin: IT I DPS 84228 Benchmark: R.L. 69.47m Source : LINZ Geodetic Database
- 3. Contractor to locate and identify existing utility location and depths prior to construction. The project engineer shall be informed of any discrepancies to the information depicted on these plans immediately for revised drawings.
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- 5. All disturbed areas shall be re-topsoiled and grassed within 30 days upon completion of contract works.

  Dust control shall be maintained until grassed
- areas have become established.

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A	11/08/23	Engineering Approval	sv	RFK	DVM
Rev	Date	Amendment	Ву	Chk	App

Project Title

Assured Construction Limited 153 Taylor Street Cambridge

Proposed Engineering Works Stormwater & Sewer Long Sections

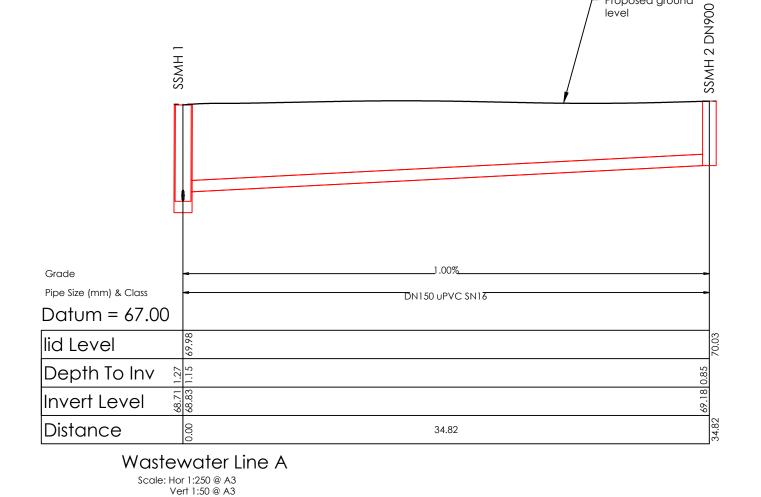
Surveyed	1	I	1
Designed	S.Pen	09/08/23	SP
Drawn	S.Veerla	09/08/23	SV
Checked	R. Kilgour	11/08/23	RFK
Approved	D.McCracken	11/08/23	DVM

Status ENGINEERING APPROVAL

Scale A1 A3

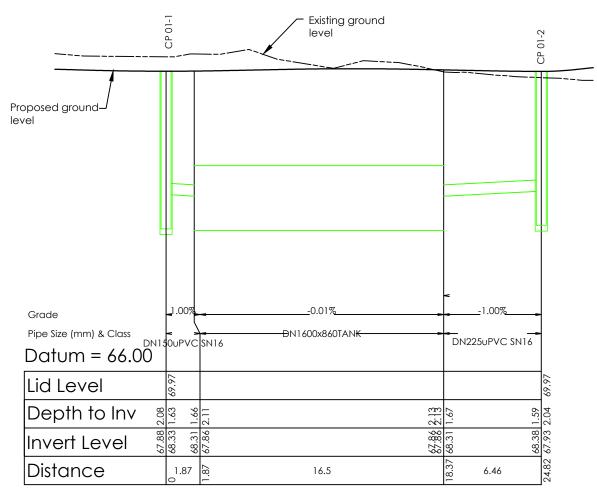
Drawing Number

230233-500

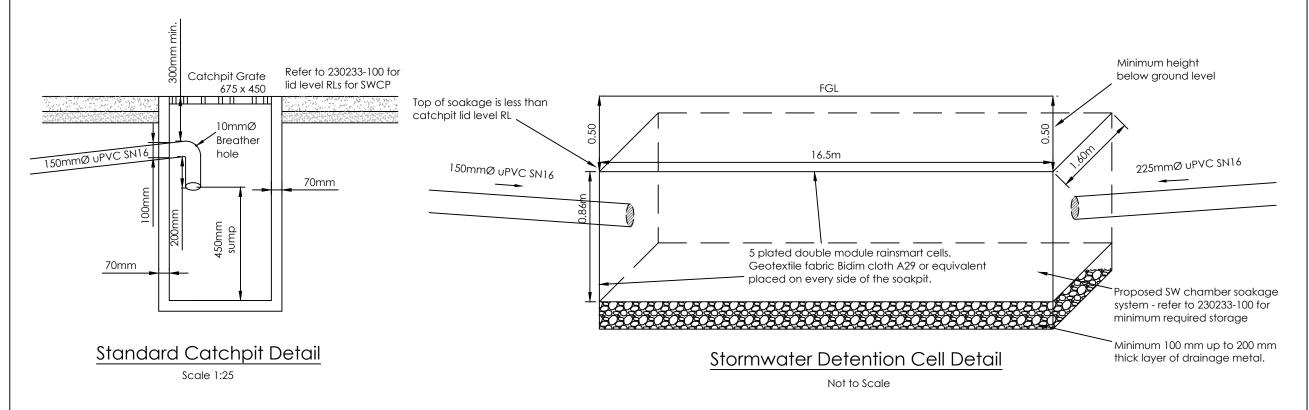


Proposed ground

level



Stormwater Line SW 01 Scale: Horz. 1:250 @ A3 Vert. 1:50 @ A3





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# NOTES:

- 1. All work shall be done in accordance with the Regional Infrastructure Technical Specifications (RITS) and approval of Waipa
- 2. Reduced Levels are in terms of Moturiki Vertical Datum 1953 Origin: IT I DPS 84228 Benchmark: R.L. 69.47m Source: LINZ Geodetic Database
- 3. Contractor to locate and identify existing utility location and depths prior to construction. The project engineer shall be informed of any discrepancies to the information depicted on these plans immediately for revised drawings.
- 4. Contractor shall install and maintain all stormwater, dust and erosion control during construction as per earthworks managemen plan and the Waikato Regional Council Erosion and Sediment Control - "Guidelines for soil disturbing activities".
- 5. All disturbed areas shall be re-topsoiled and grassed within 30 days upon completion of Dust control shall be maintained until grassed areas have become established.

Α	17/08/23	Engineering Approval	SV	RFK	DVN
Rev	Date	Amendment	Ву	Chk	Apı

Project Title

Assured Construction Limited 153 Taylor Street Cambridge

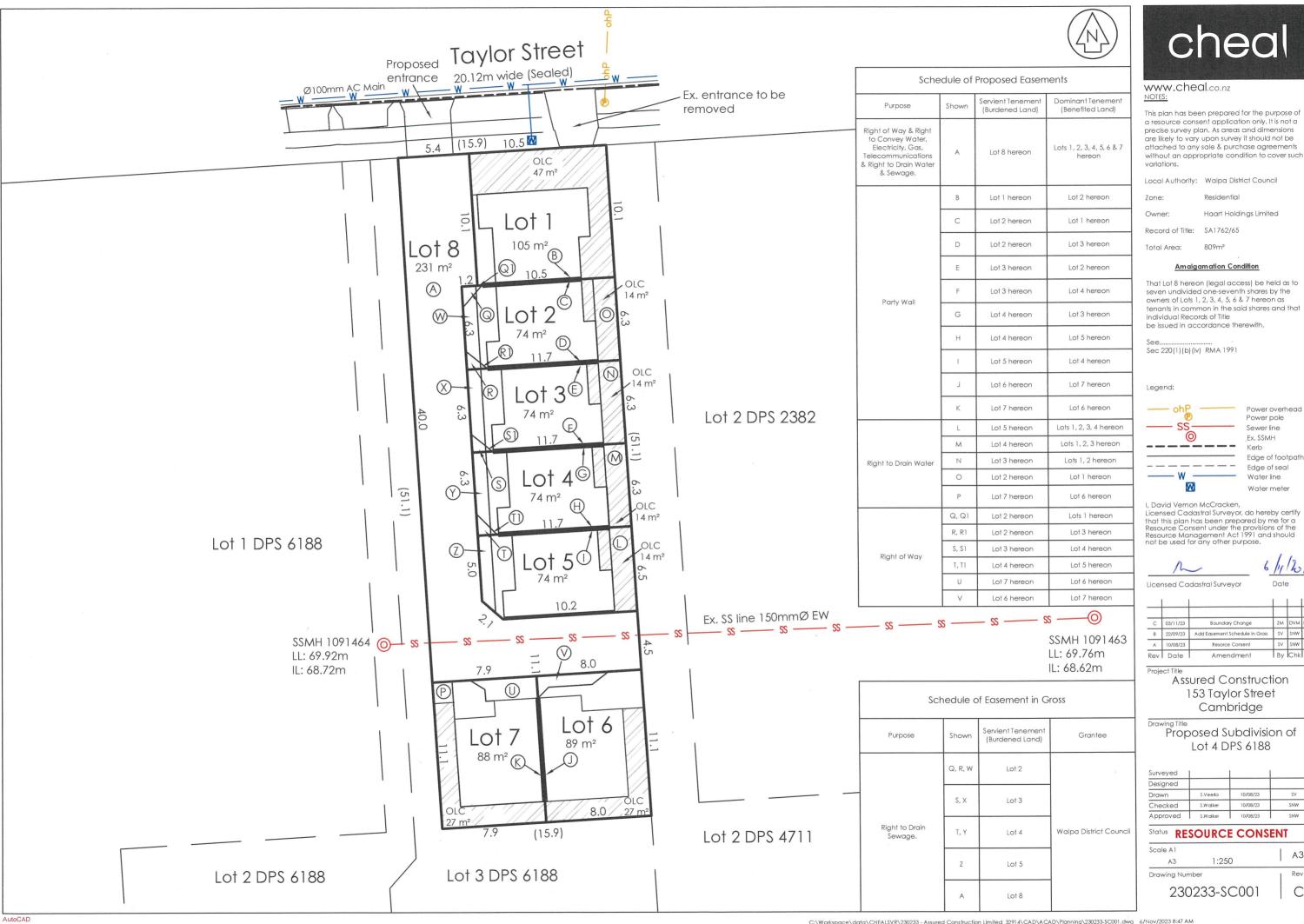
Proposed Engineering Works Stormwater Details

Surveyed			
Designed	S.Pen	09/08/23	SP
Drawn	S.Veerla	09/08/23	SV
Checked	R. Kilgour	11/08/23	RFK
Approved	D.McCracken	17/08/23	DVM

# Status ENGINEERING APPROVAL

Scale A1		د ۸ ا
A3	1:250	A3
Drawing Number	ı Rev	

230233-600



Rev